

SFMTA - TASC SUMMARY SHEET

PreStaff_Date: 4/14/2026 Requested_by: SFPW Handled: Sam Lam 646-2479 Section Head : B. Dusseault	<input type="checkbox"/> Public Hearing Consent <input checked="" type="checkbox"/> Public Hearing Regular <input type="checkbox"/> Informational / Other <small>PH - Regular</small>	No objections: _____ Item Held: _____ Other: _____
Location: Powell Street between Geary and Ellis Streets		
Subject: Street Improvements		
PROPOSAL / REQUEST: Proposal to establish parking restrictions and consolidate cable car stops as part of the Powell Street Improvement Project. RESCIND – TOW-AWAY, NO STOPPING, ANY TIME, EXCEPT COMMERCIAL LOADING, 30 MINUTE TIME LIMIT ESTABLISH – TOW-AWAY, NO STOPPING, ANY TIME Powell Street, west side, from 84 feet to 174 feet north of Ellis Street Powell Street, east side, from 92 feet to 152 feet north of Ellis Street Powell Street, east side, from 97 feet to 177 feet north of O’Farrell Street RESCIND – WHITE ZONE, PASSENGER LOADING, AT ALL TIMES ESTABLISH – TOW-AWAY, NO STOPPING, ANY TIME Powell Street, west side, from 89 feet to 149 feet north of O’Farrell Street RESCIND – TOW-AWAY, NO STOPPING, ANY TIME ESTABLISH – TOW-AWAY, NO STOPPING, ANY TIME EXCEPT COMMERCIAL LOADING, 30 MINUTE TIME LIMIT Powell Street, west side, from 22 feet to 82 feet north of Ellis Street		
BACKGROUND INFORMATION / COMMENTS Item scope amended at 4/14/2026 Prestaff. See attached for full background. Please note that while the project includes string lights over Powell Street, review and approval is not requested for the string lights at this time, and it will be brought to TASC as an informational item at a later date if necessary. Current plans for the string lights are attached for informational purposes only. The Powell Street Improvement Project will improve Powell Street between Market to Geary Streets by widening sidewalks, improving cable car access, landscaping, and adding lighting elements along Powell Street. The project is included in Mayor Daniel Lurie's "Heart of the City" Executive Directive to help revitalize downtown San Francisco and to accelerate the City's economic comeback. The project is currently led by SF Public Works on the city side, and design is led by landscape architect consultants Field Operations. Project improvements will include removal of the existing parklets, widen existing sidewalks by 5’ (from 15’ to 20’) except at proposed commercial loading zones, installing catenary lighting along Powell Street, a light sculpture called the “Golden Lantern” just south of the cable car turnaround, and various decorative landscaping elements. At this time, we are requesting only the review and approval of the parking changes and cable car stop consolidations. If necessary for review and approval, string lights will be brought to TASC at a later date. Plans for the string lights are included for informational purposes only.		
HEARING NOTIFICATION AND PROCESSING NOTES:		ENVIRONMENTAL CLEARANCE BY: <input type="checkbox"/> SFMTA <input checked="" type="checkbox"/> Attached <input type="checkbox"/> Pending
CHECK IF PREPARING SEPARATE SFMTA BOARD CALENDAR ITEM FOR PROPOSAL: <input type="checkbox"/>		

Powell Street Improvements

PreStaff Date: 4/14/2026

Location: Powell Street between Geary Street and Ellis Street

Subject: Street Improvements

Description:

Item scope amended at 4/14/2026 Prestaff. Changes in red in background section.

RESCIND – TOW AWAY NO STOPPING ANY TIME, EXCEPT COMMERCIAL LOADING, 30 MINUTE LIMIT

ESTABLISH – TOW AWAY NO STOPPING ANYTIME

Powell Street, west side from 84 feet to 174 feet north of Ellis Street

Powell Street, east side, from 92 feet to 152 feet north of Ellis Street

Powell Street, east side, from 97 feet to 177 feet north of O'Farrell Street

RESCIND – PASSENGER LOADING ZONE AT ALL TIMES

ESTABLISH – TOW AWAY NO STOPPING ANYTIME

Powell Street, west side, from 89 feet to 149 feet north of O'Farrell Street

RESCIND – TOW AWAY NO STOPPING ANYTIME

ESTABLISH – TOW AWAY NO STOPPING ANYTIME EXCEPT COMMERCIAL LOADING, 30 MINUTE LIMIT

Powell Street, west side, from 22 feet to 82 feet north of Ellis Street

Powell Street, east side, from 20 feet to 80 feet south of O'Farrell Street

Powell Street, west side, from 13 feet to 68 feet south of Geary Street

Powell Street, east side, from 21 feet to 78 feet south of Geary Street

ESTABLISH – TOW AWAY NO STOPPING ANYTIME

Powell Street, west side, from Geary Street to 13 feet southerly

RESCIND – CABLE CAR STOP

Powell Street, east side, south of O'Farrell Street

Powell Street, east side, south of Geary Street

Powell Street, west side, north of O'Farrell Street

Powell Street, west side, north of Geary Street

ESTABLISH – CABLE CAR STOP

Powell Street, east side, midblock between O'Farrell Street and Geary Street

Powell Street, west side, midblock between O'Farrell Street and Geary Street

(Supervisor District 3)

Background:

The Powell Street Improvement Project will improve Powell Street between Market to Geary Streets by widening sidewalks, improving cable car access, landscaping, and adding lighting elements along Powell Street. The project is included in Mayor Daniel Lurie's "Heart of the City" Executive Directive to help revitalize downtown San Francisco and to accelerate the City's economic comeback. The project is currently led by SF Public Works on the city side, and design is led by landscape architect consultants Field Operations.

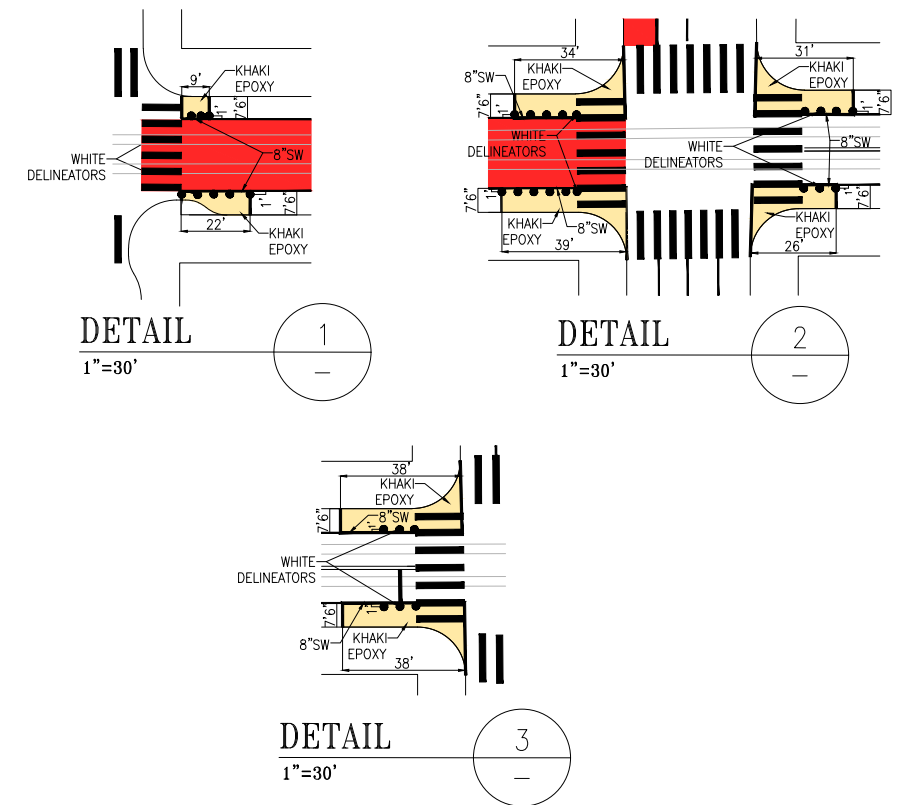
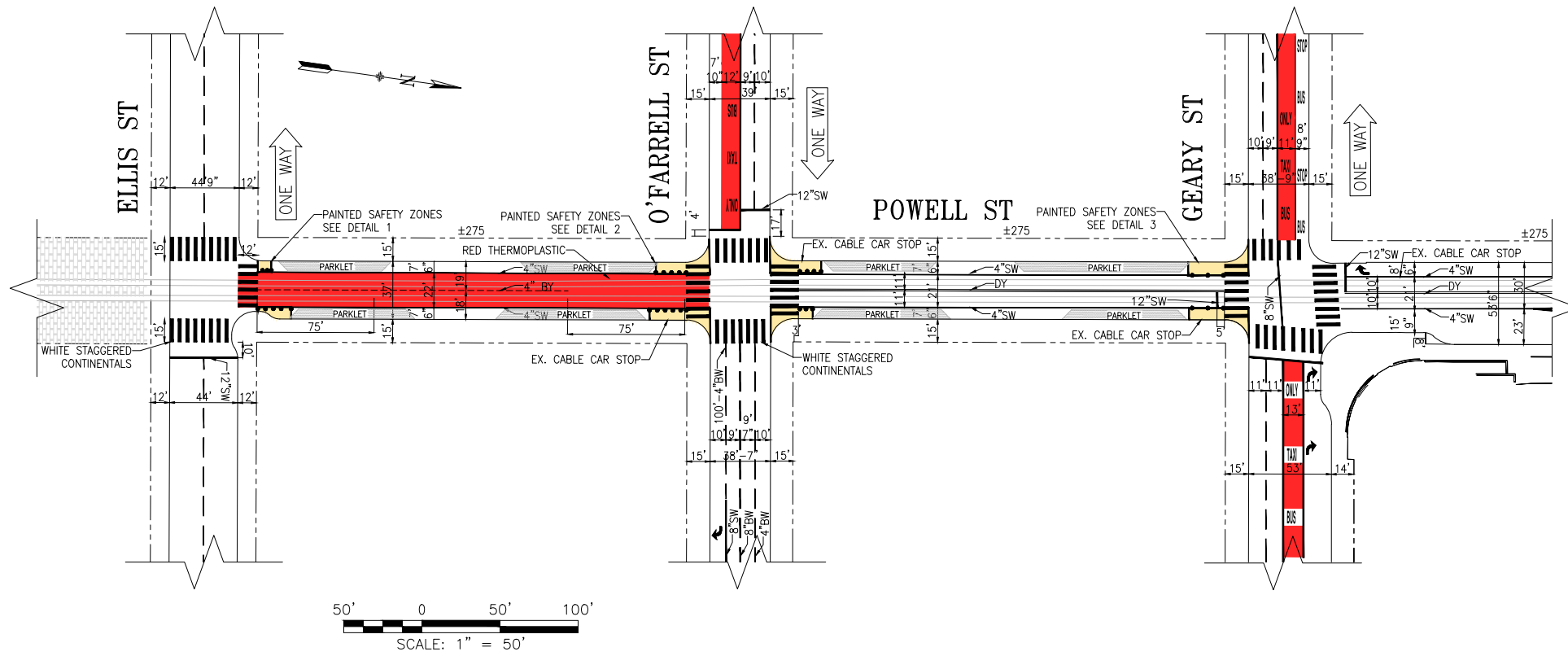
Project improvements will include removal of the existing parklets, widen existing sidewalks by 5' (from 15' to 20') except at proposed commercial loading zones, installing catenary lighting along Powell Street, a light sculpture called the "Golden Lantern" just south of the cable car turnaround, and various decorative landscaping elements.

At this time, we are requesting only the review and approval of the parking changes and cable car stop consolidations. If necessary for review and approval, string lights will be brought to TASC at a later date. Plans for the string lights are included for informational purposes only.

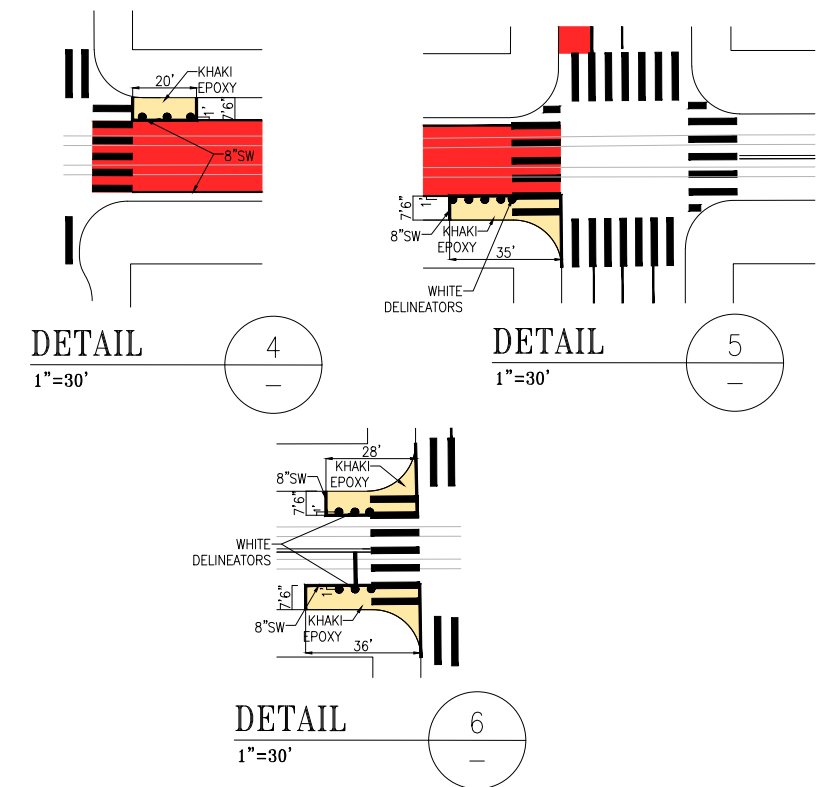
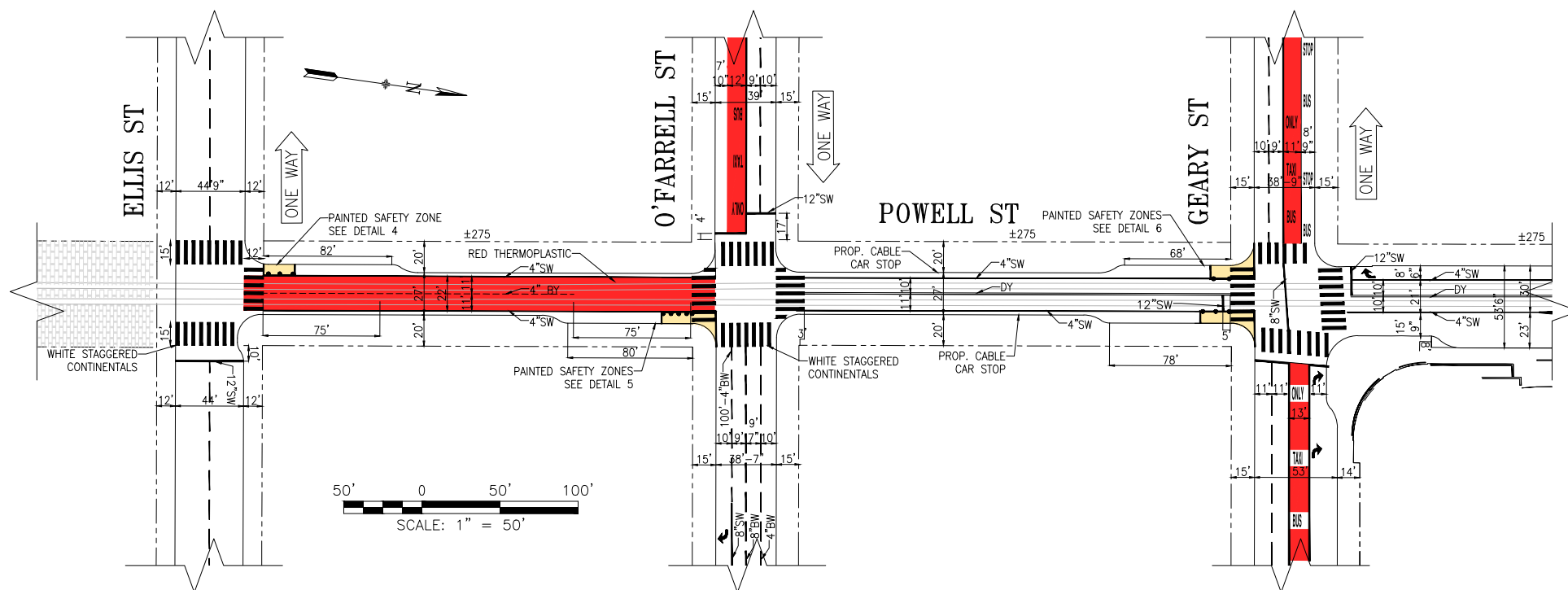
The project is also proposing to consolidate cable car stops in both directions. Cable car stops at the Powell Street/Geary Street and the Powell Street/O'Farrell Street intersections would be consolidated into one midblock cable car stop per direction on Powell Street between Geary Street and O'Farrell Street. This change is proposed by SFMTA staff as a response to the proposed commercial loading zone locations, which is primarily driven by utility conflicts. The project is widening sidewalks where feasible, and commercial loading zones are proposed where utility conflicts make sidewalk widening infeasible.

The Powell Street Improvement Project follows the improvements with the Powell Safety project. The Powell Safety project is expected to finish construction in Summer 2026. Parking legislation for the Powell Safety project was passed in 2020 and originally included pedestrian bulbouts at all three intersections, but most bulbouts were removed from that project.

EXISTING CONDITIONS:



PROPOSED CONDITIONS:



NO.	DATE	DESCRIPTION	BY	APP
TABLE OF REVISIONS				
CHECK WITH TRACING TO SEE IF YOU HAVE LATEST REVISION				



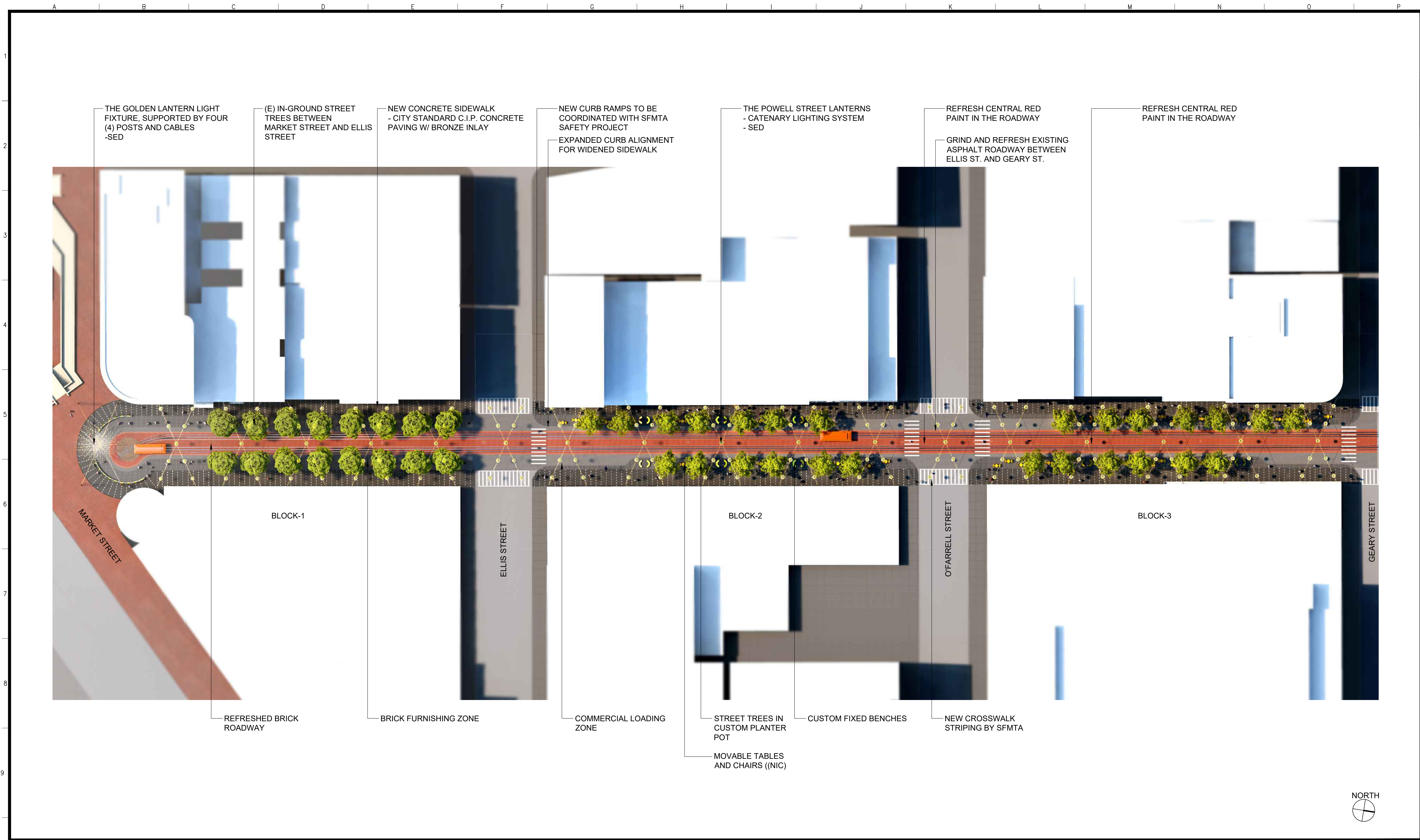
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CHECKED: D.VALLE-SCHWENK		DATE: 3/2026	
APPROVED: SENIOR ENGINEER		DATE:	
APPROVED: CITY TRAFFIC ENGINEER		DATE:	

SCALE:
1" = 50'
SHEET/SHEETS:
1 OF 1

POWELL STREET IMPROVEMENTS PROJECT
EXISTING & PROPOSED TRAFFIC STRIPING
POWELL STREET
ELLIS STREET TO POST STREET

SOURCING EVENT ID.
DRAWING NO.
FILE NO.
REV. NO.
0

FILE NAME:
DATE:



NO.	DATE	DESCRIPTION	BY	APP.
TABLE OF REVISIONS				

REFERENCE INFORMATION & FILE NO. OF SURVEYS



BUREAU OF LANDSCAPE ARCHITECTURE
 CITY & COUNTY OF SAN FRANCISCO
SAN FRANCISCO PUBLIC WORKS
 49 SOUTH VAN NESS AVENUE, SUITE 1000
 SAN FRANCISCO, CA 94103

Section Mgr:	TONY ESTERBROOKS	Date:	
Bureau Mgr:	JENNIFER COOPER	DESIGNED:	DATE:
		DRAWN:	DATE:
		CHECKED:	DATE:

NOT FOR CONSTRUCTION

SCALE:
SHEET OF SHEETS
41/170

POWELL STREETSCAPE IMPROVEMENTS PROJECT

ILLUSTRATIVE SITE PLAN

LANDSCAPE ARCHITECT:

 Field Operations
 649 Front Street, San Francisco, CA 94111

SOURCING EVENT ID.
00000XXXX
 DRAWING NO.
L-100
 FILE NO.
 REV. NO.



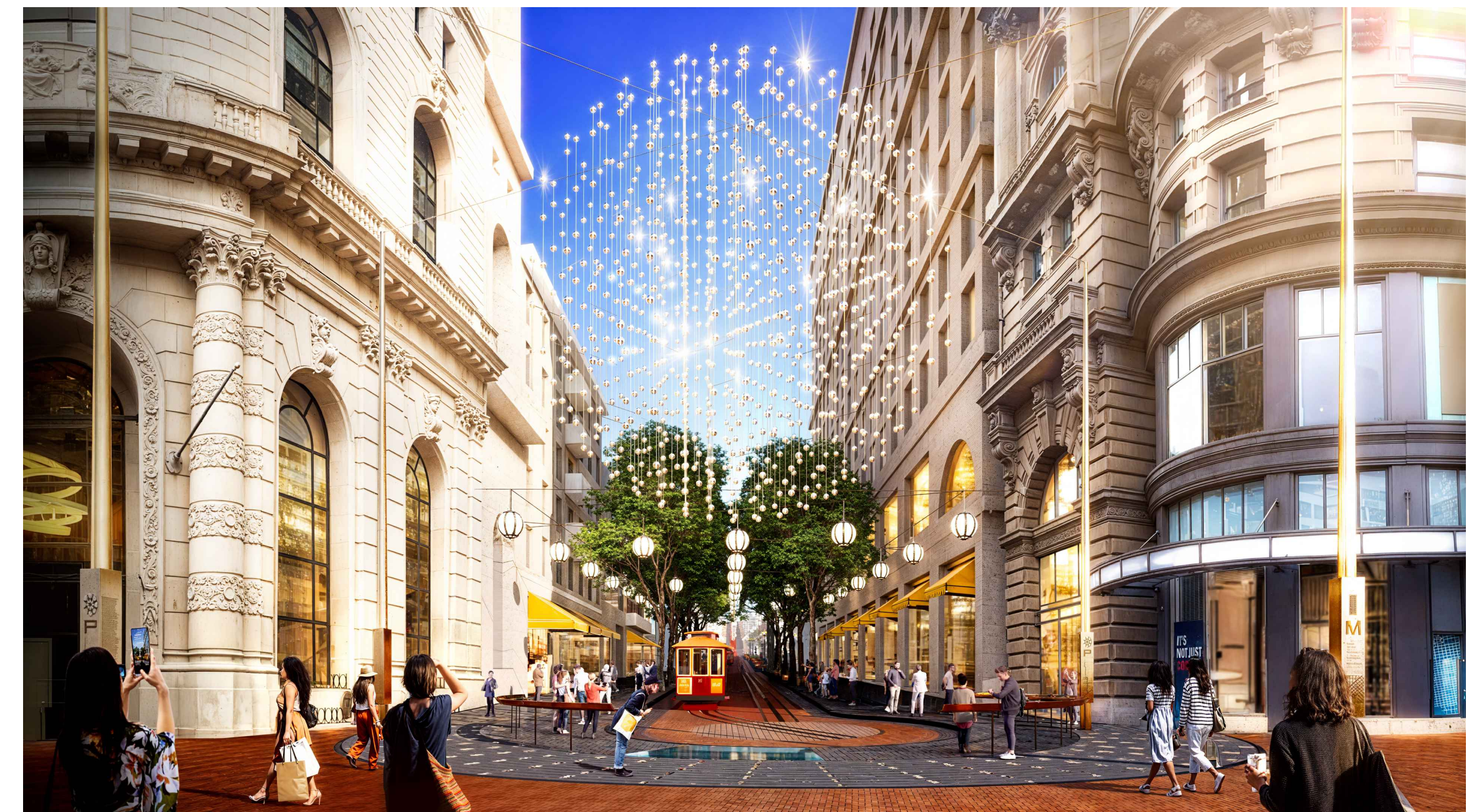
04 THE GOLDEN LANTERN @ MARKET ST & POWELL ST - GAME DAY
ILLUSTRATIVE VIEW NTS



02 THE GOLDEN LANTERN @ MARKET ST & POWELL ST - NIGHT VIEW
ILLUSTRATIVE VIEW NTS



03 THE GOLDEN LANTERN @ MARKET ST & POWELL ST - PRIDE EVENT
ILLUSTRATIVE VIEW NTS



01 THE GOLDEN LANTERN @ MARKET ST & POWELL ST - DAY VIEW
ILLUSTRATIVE VIEW NTS

NO.	DATE	DESCRIPTION	BY	APP.
TABLE OF REVISIONS				

REFERENCE INFORMATION
& FILE NO. OF SURVEYS



BUREAU OF LANDSCAPE ARCHITECTURE
CITY & COUNTY OF SAN FRANCISCO
SAN FRANCISCO PUBLIC WORKS
49 SOUTH VAN NESS AVENUE, SUITE 1000
SAN FRANCISCO, CA 94113

Section Mgr:	TONY ESTERBROOKS
Bureau Mgr:	JENNIFER COOPER

Date:	DESIGNED: DATE:
	DRAWN: DATE:
	CHECKED: DATE:
NOT FOR CONSTRUCTION	

SCALE:
SHEET OF SHEETS
42/170

POWELL STREETSCAPE IMPROVEMENTS PROJECT
VISUALIZATION

LANDSCAPE ARCHITECT:
fo
Field Operations
649 Front Street, San Francisco, CA 94111

SOURCING EVENT ID.
000000XXXX
DRAWING NO.
L-101
FILE NO.
REV. NO.



04 MARKET ST & POWELL ST HOLIDAY VIEW
ILLUSTRATIVE VIEW

NTS



02 POWELL ST MID-BLOCK NIGHT VIEW
ILLUSTRATIVE VIEW

NTS



03 POWELL ST MID-BLOCK EVENT VIEW
ILLUSTRATIVE VIEW

NTS



01 POWELL ST MID-BLOCK DAY VIEW
ILLUSTRATIVE VIEW

NTS

NO.	DATE	DESCRIPTION	BY	APP.
TABLE OF REVISIONS				

REFERENCE INFORMATION
& FILE NO. OF SURVEYS



BUREAU OF LANDSCAPE
ARCHITECTURE
CITY & COUNTY OF SAN FRANCISCO
SAN FRANCISCO PUBLIC WORKS
49 SOUTH VAN NESS AVENUE, SUITE 1000
SAN FRANCISCO, CA 94103

Section Mgr:	TONY ESTERBROOKS	Date:	
Bureau Mgr:	JENNIFER COOPER		

DESIGNED:	DATE:
DRAWN:	DATE:
CHECKED:	DATE:

NOT FOR
CONSTRUCTION

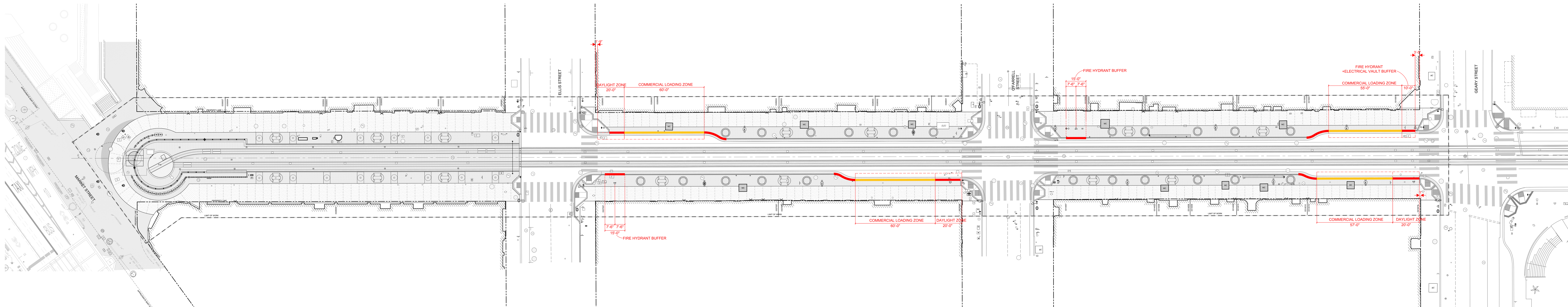
SCALE:
SHEET OF SHEETS
43/170

POWELL STREETSCAPE
IMPROVEMENTS PROJECT

VISUALIZATION



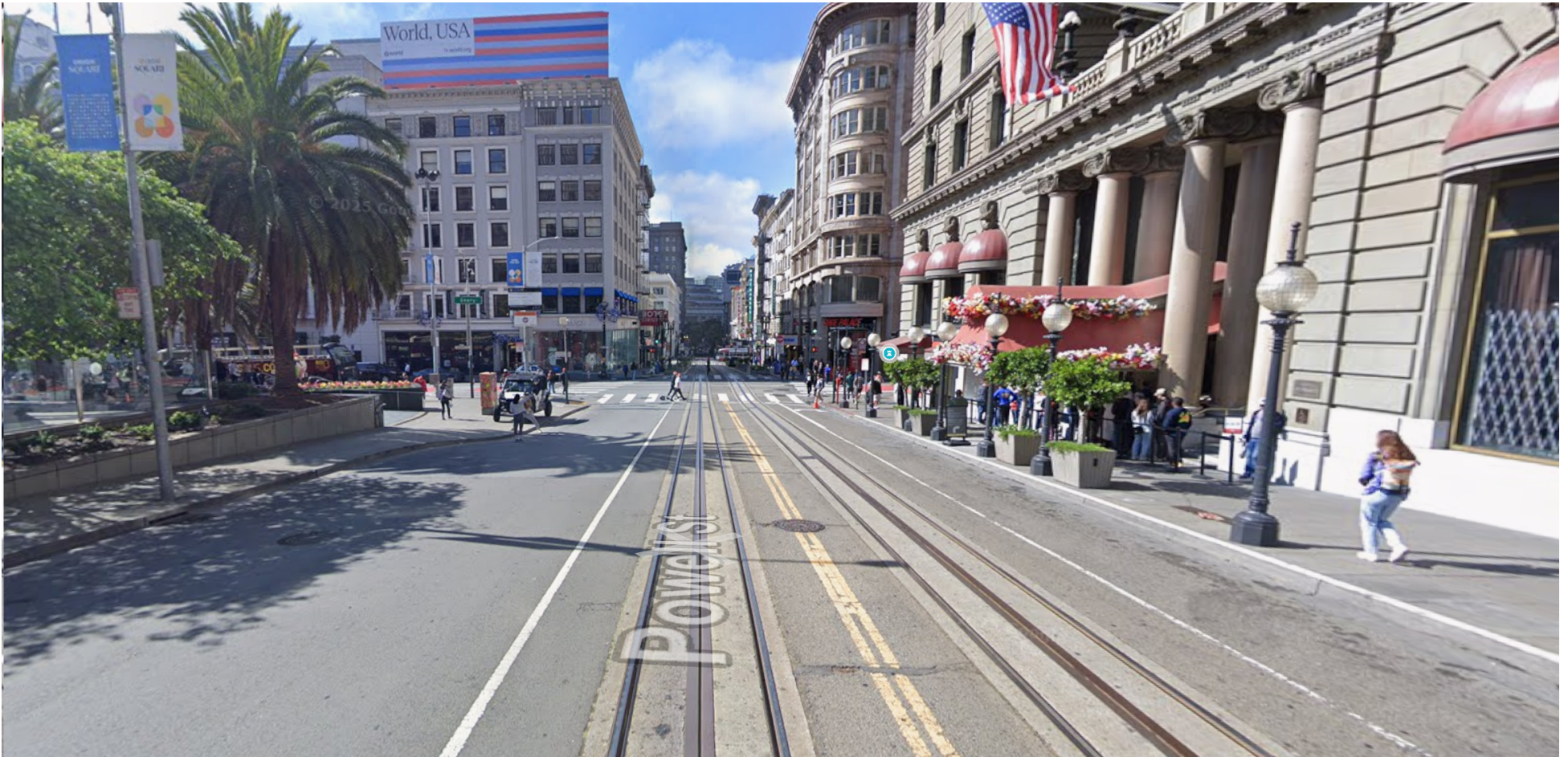
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FILE NO.	
REV. NO.	



— RED PAINTED CURB PER SFMTA
— YELLOW PAINTED CURB PER SFMTA

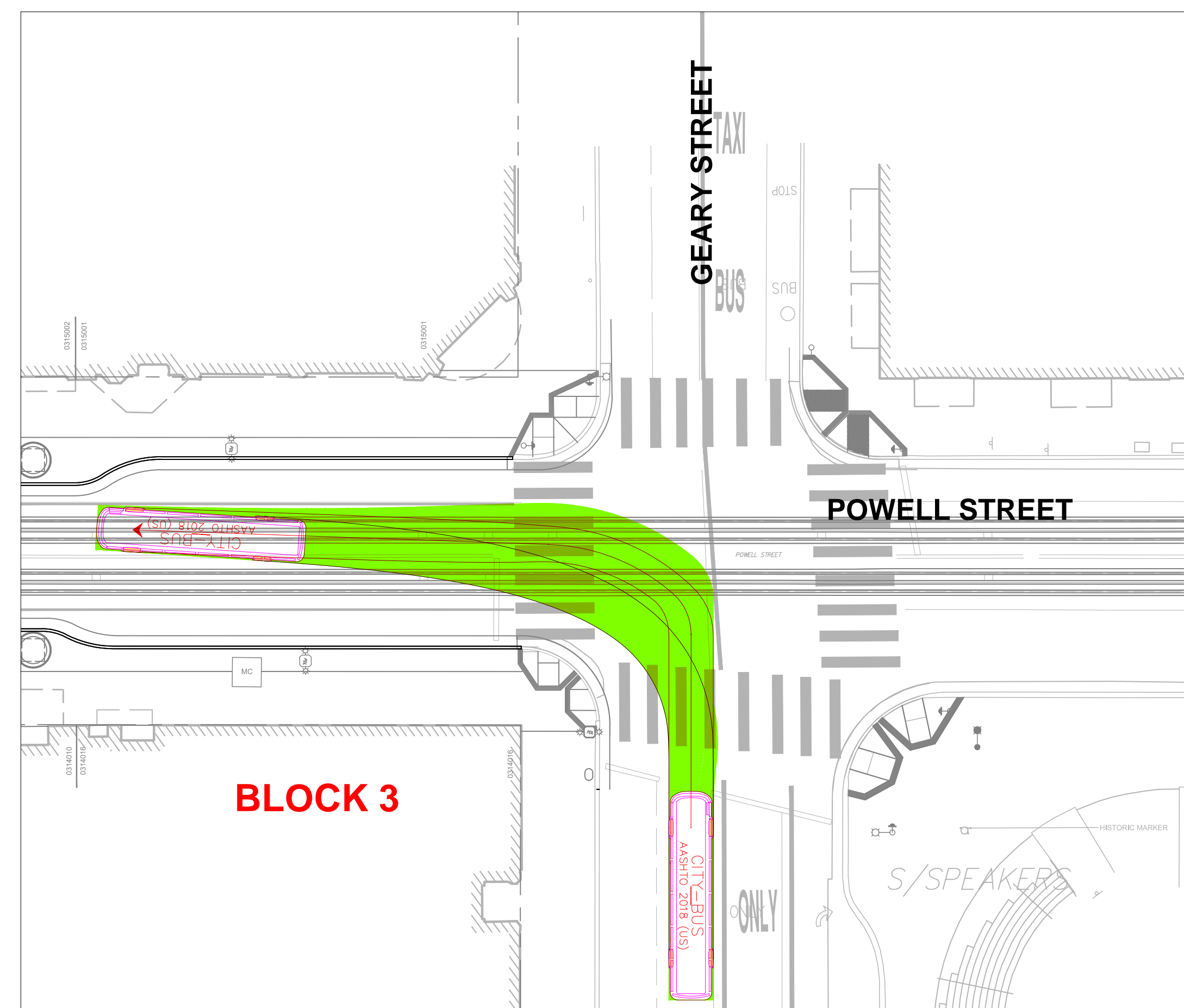
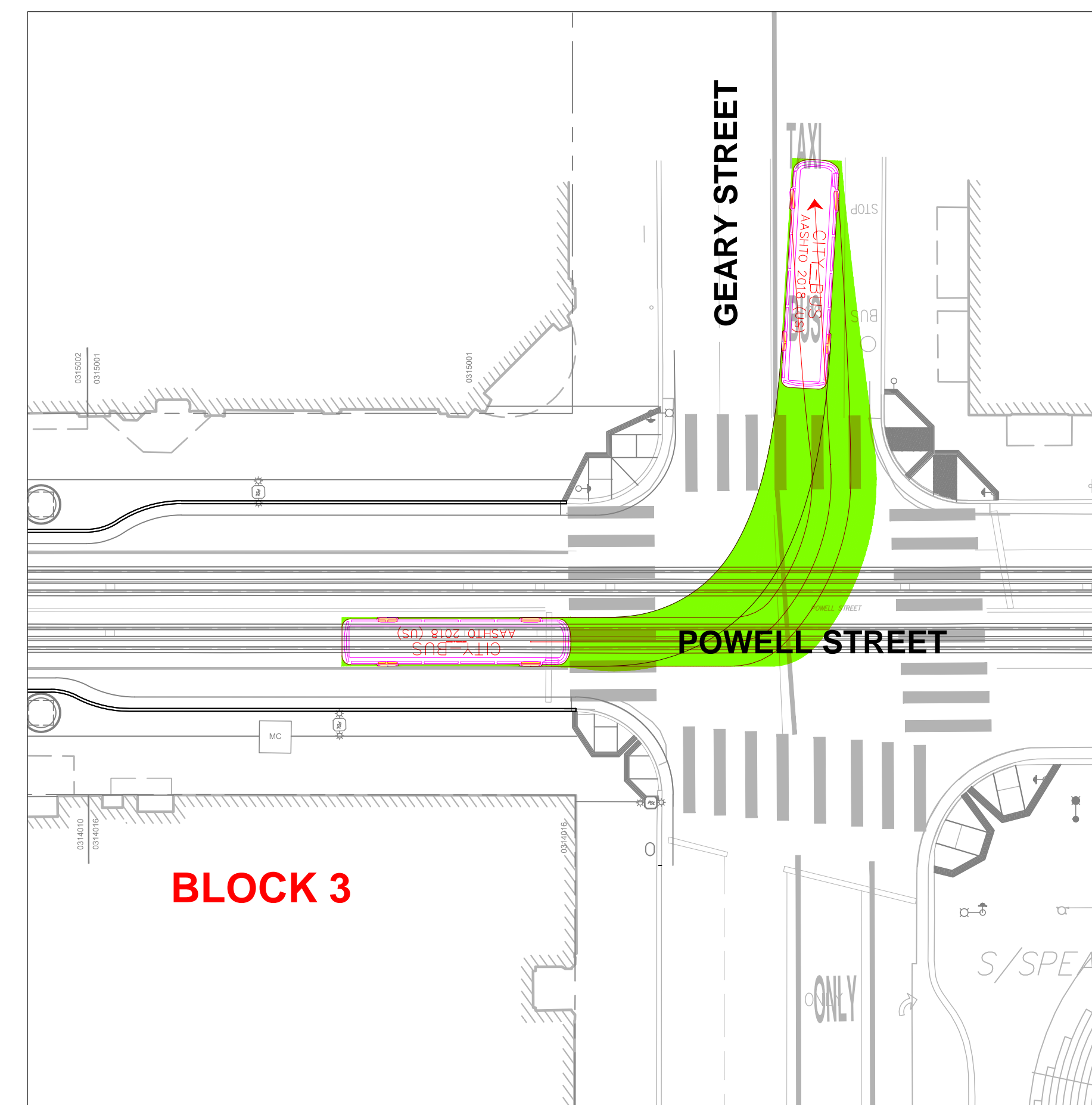
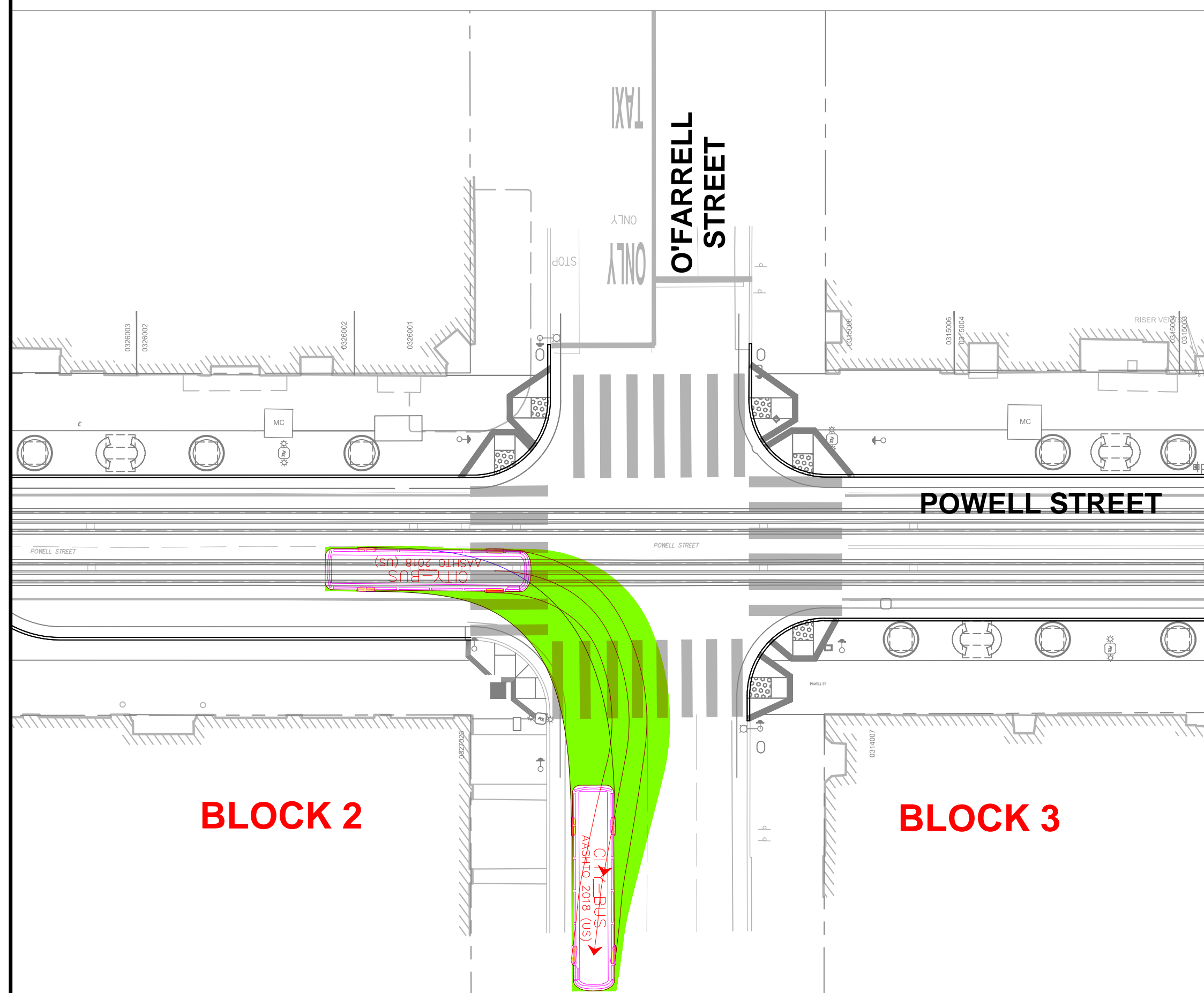
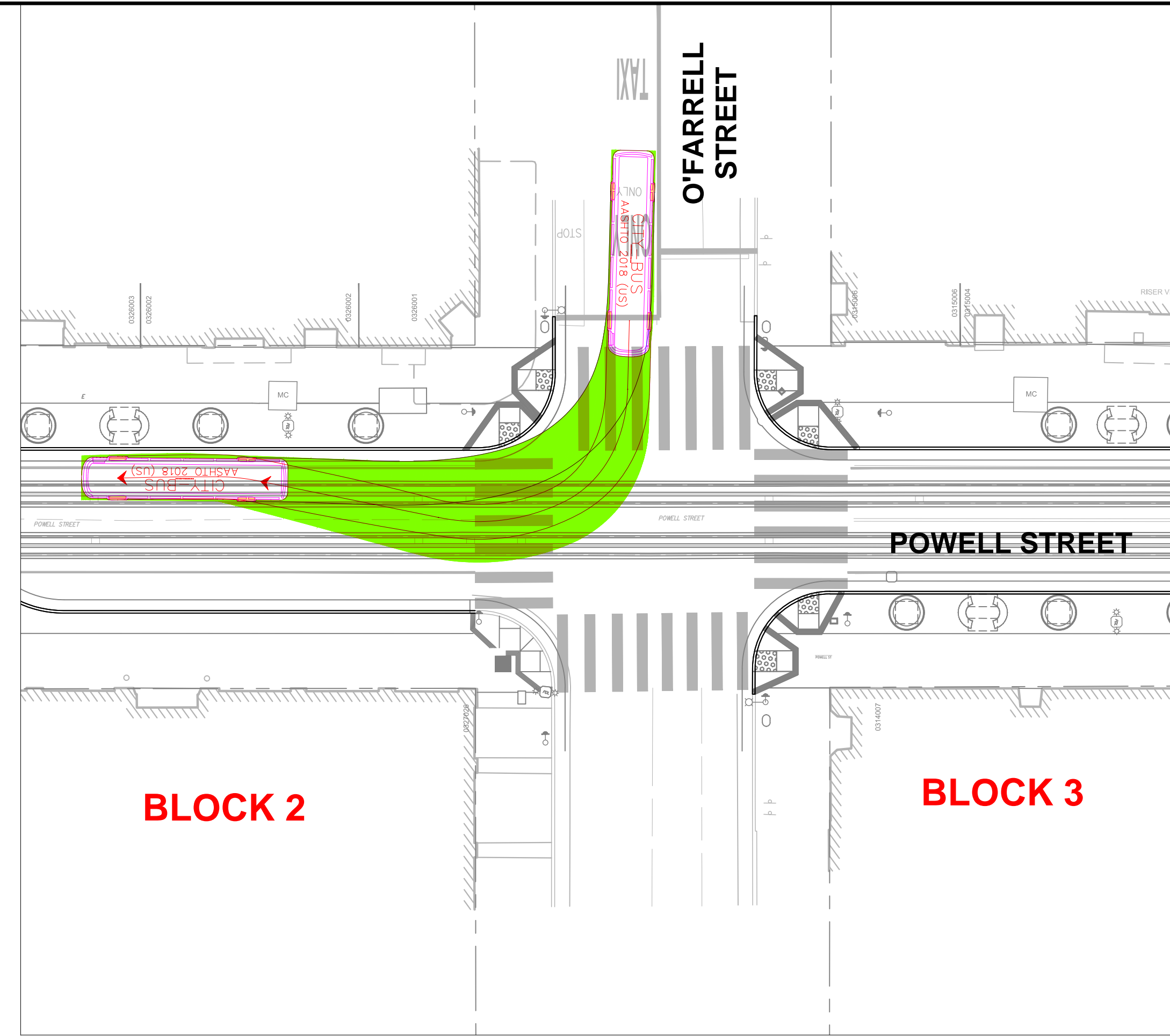
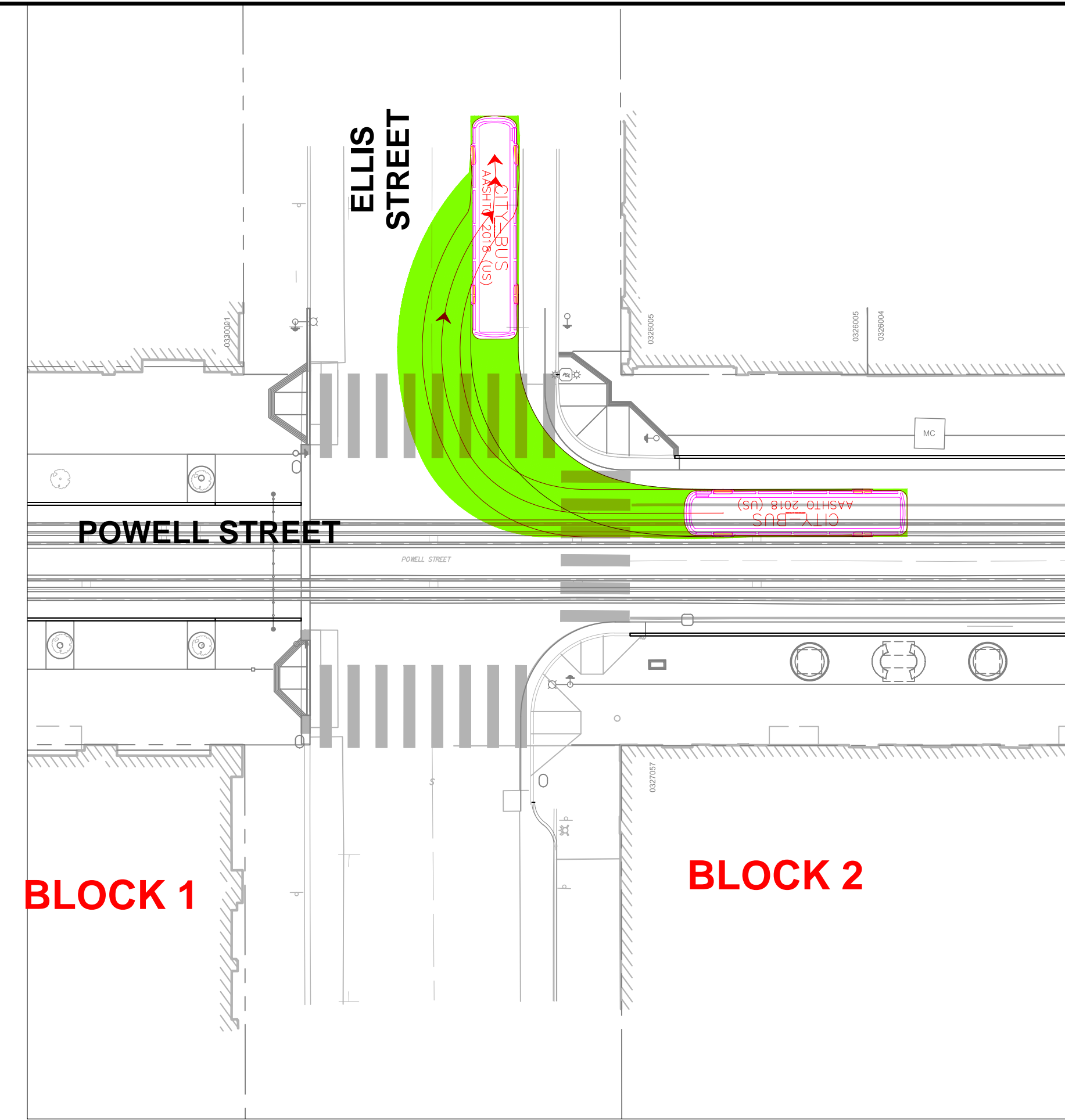
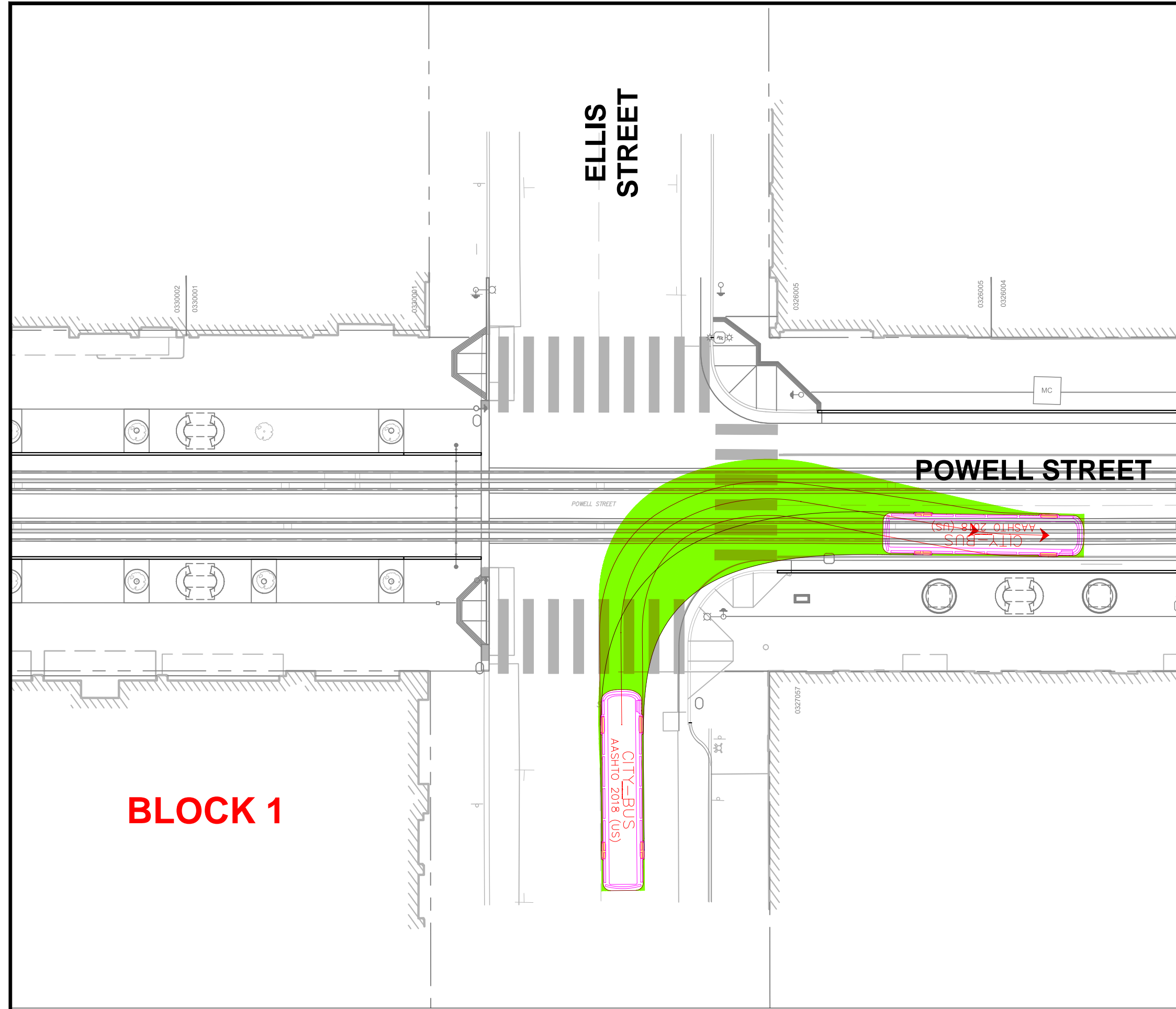
Powell Curb Color Diagram
03/20/26

Powell St, looking south, from Post to Geary



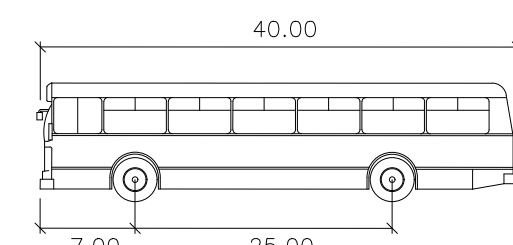
Powell St, looking north, at Ellis



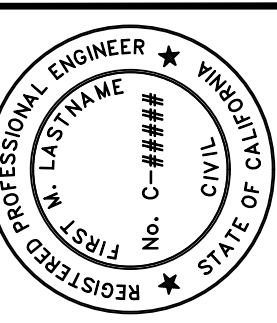
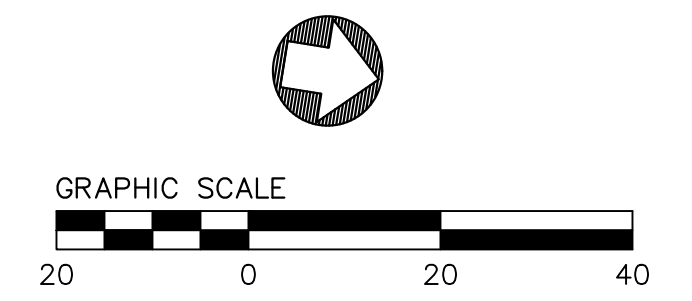


LEGEND:

- DIRECTION OF TRAVEL
- REAR TIRE EXTERIOR LIMIT
- FRONT TIRE EXTERIOR LIMIT
- VEHICLE BODY TURNING ENVELOPE

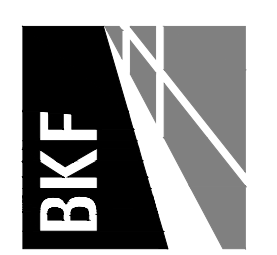


CITY-BUS TRUCK TURNING EXHIBIT



PRELIMINARY
NOT FOR CONSTRUCTION

DATE: 03/19/2026
FIRST M. LASTNAME C-####



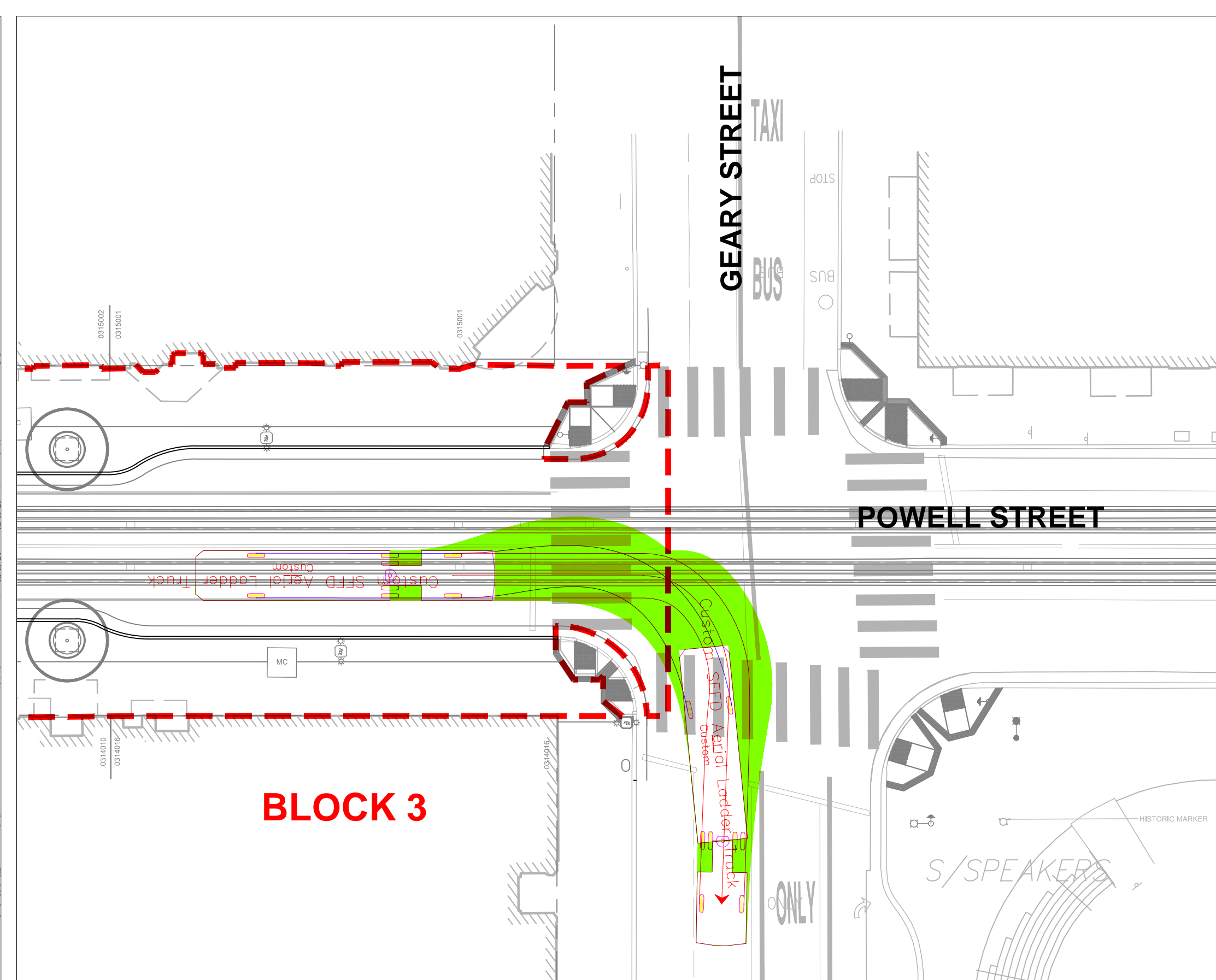
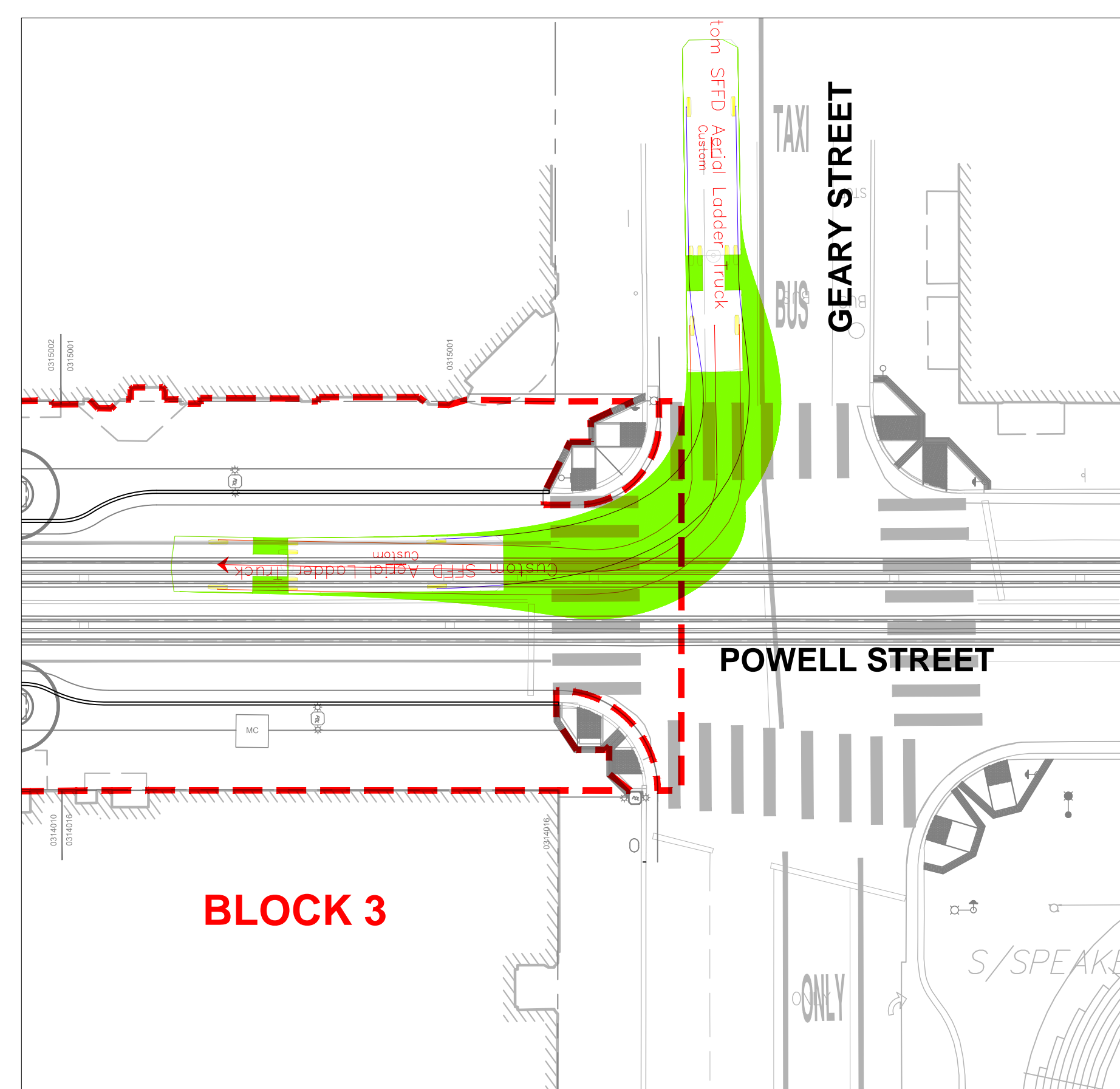
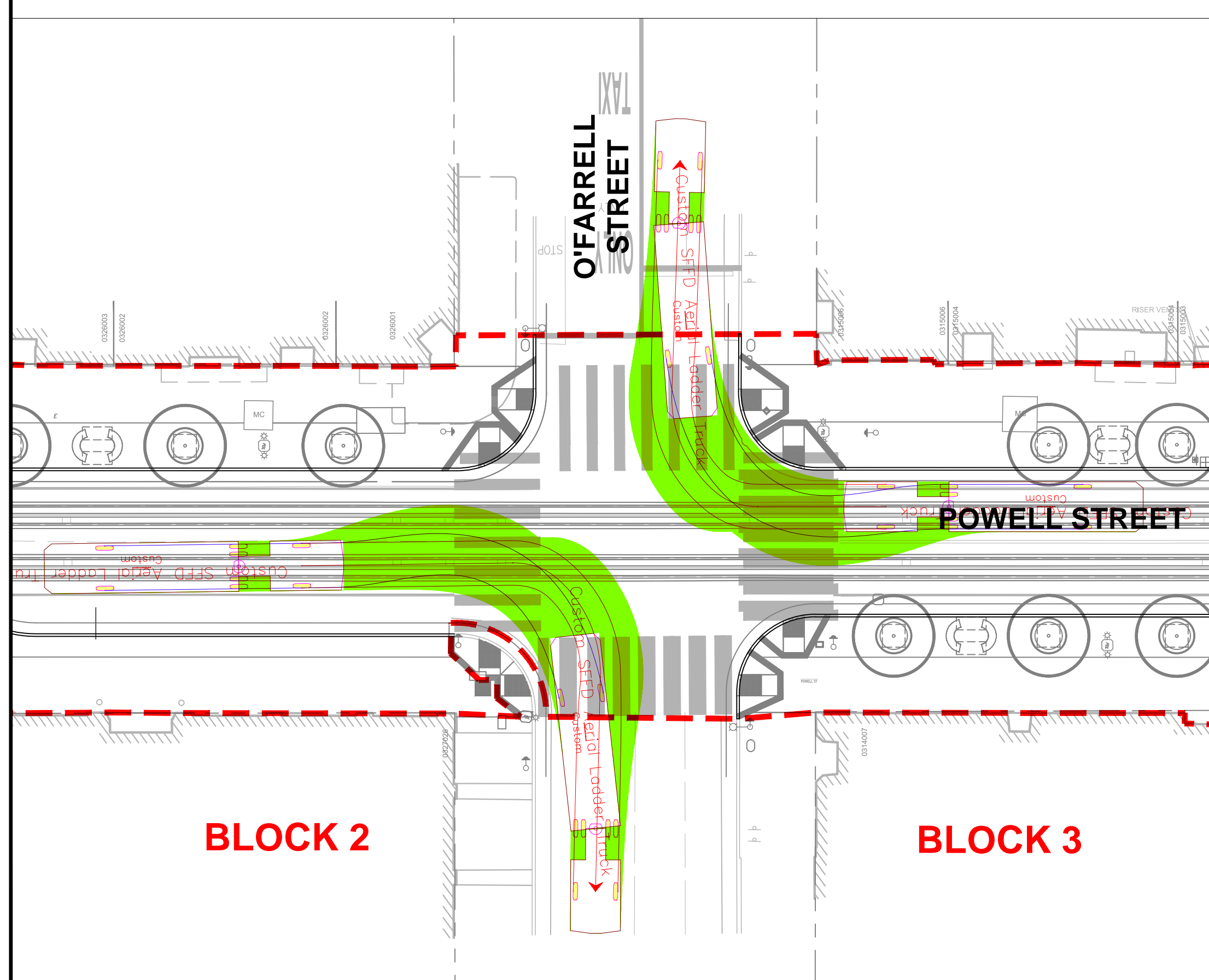
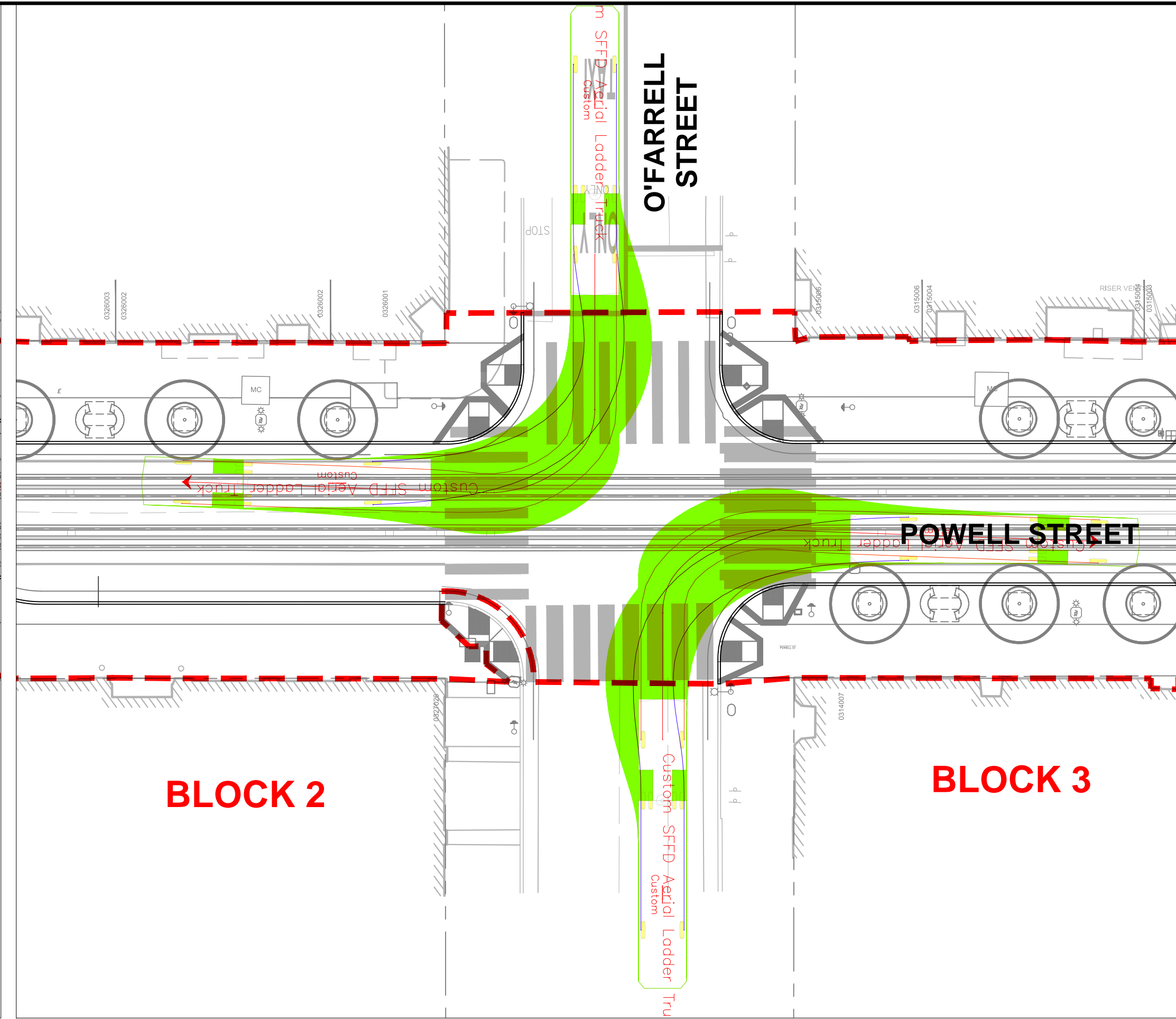
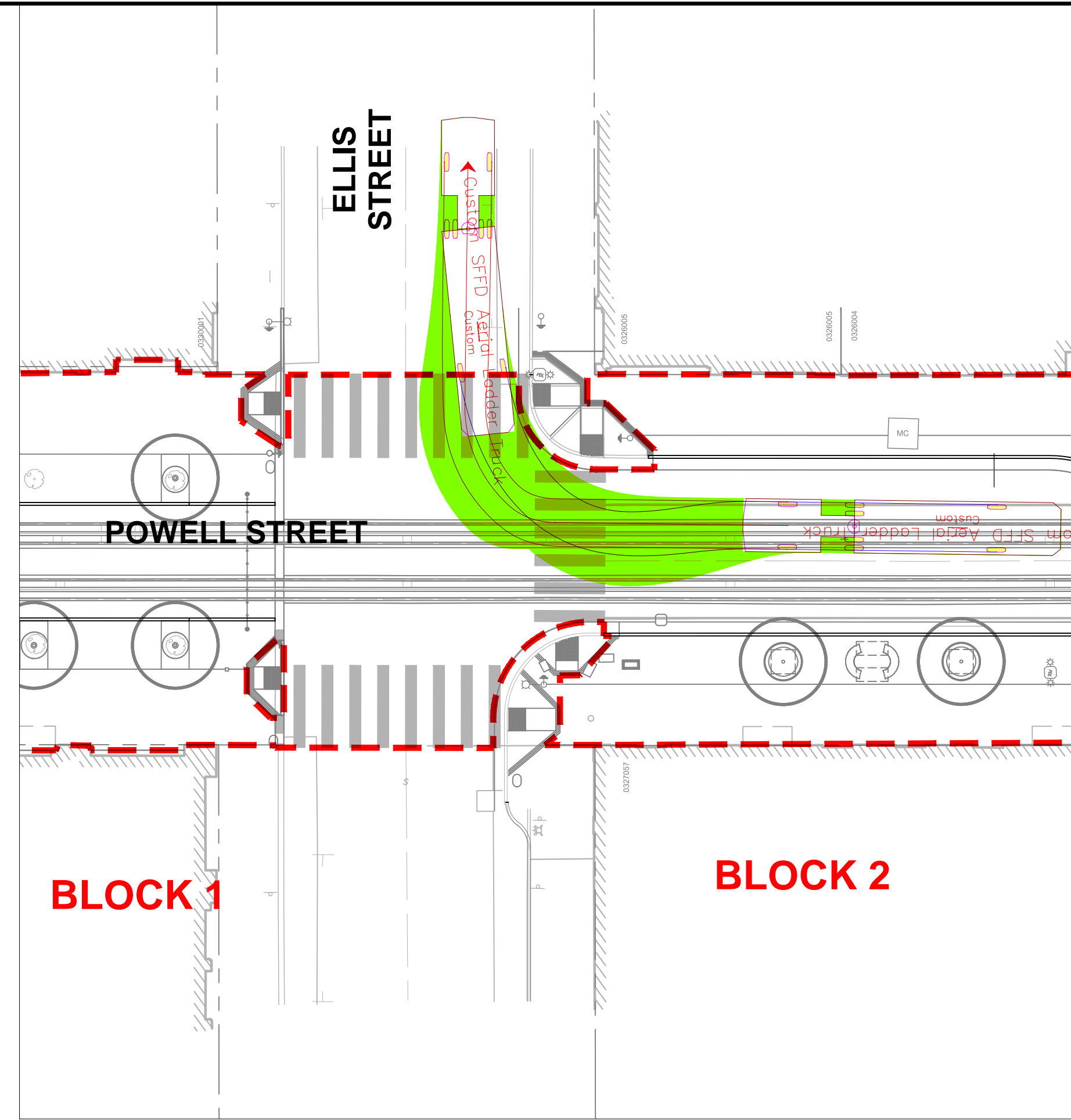
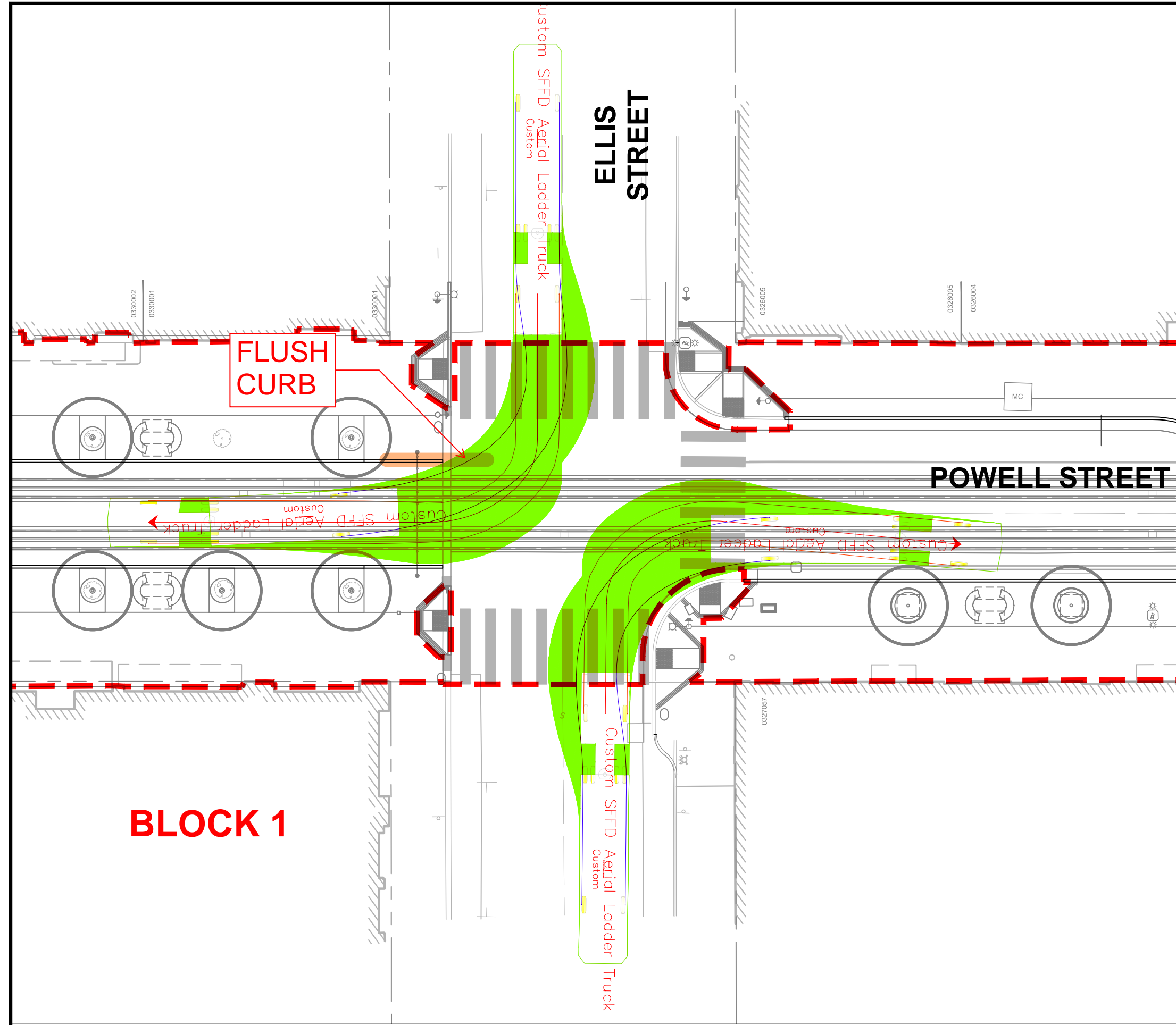
PROJECT NAME
APN 000-000-000
STREET NUMBER AND NAME, CITY/COUNTY

TRUCK TURNING EXHIBIT

No.	Date	Revisions

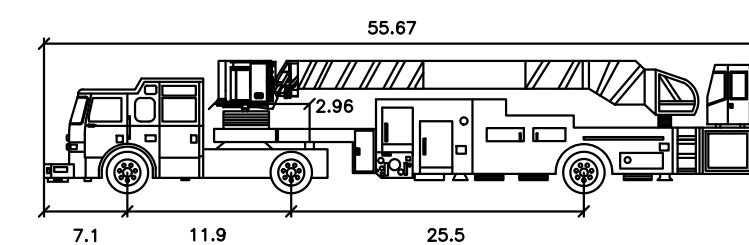
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Design: DESN
Drawn: DRWN
Approved: APRV
Job No.: JOB NO

Drawing Number:
EXH 2
2 OF 7



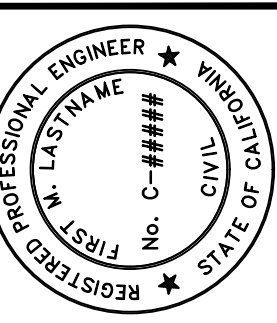
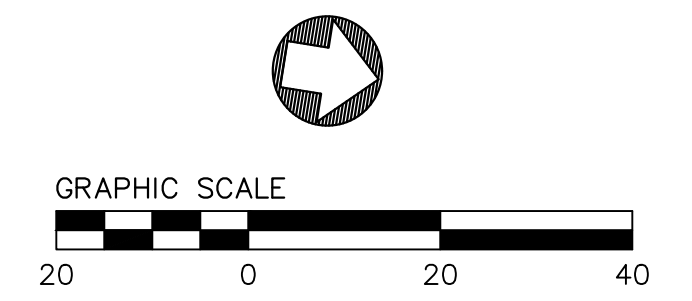
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- DIRECTION OF TRAVEL
- REAR TIRE EXTERIOR LIMIT
- FRONT TIRE EXTERIOR LIMIT
- VEHICLE BODY TURNING ENVELOPE



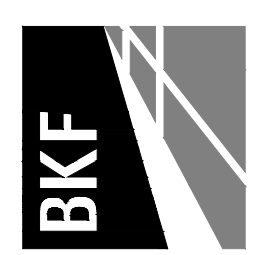
SAN FRANCISCO FIRE TRUCK
 feet
 First Unit Width : 8.00 Lock to Lock Time : 6.0
 Trailer Width : 8.00 Steering Angle : 45.0
 First Unit Track : 8.00 Articulating Angle : 70.0
 Trailer Track : 8.00

FIRE TRUCK TURNING EXHIBIT



PRELIMINARY
 NOT FOR CONSTRUCTION
 DATE: 03/19/2026
 FIRST M. LASTNAME C-####

BKF ENGINEERS
 7001 STORRIDGE DRIVE
 SUITE 360
 PLEASANTON, CA 94588
 (925) 396-7700
 www.bkf.com

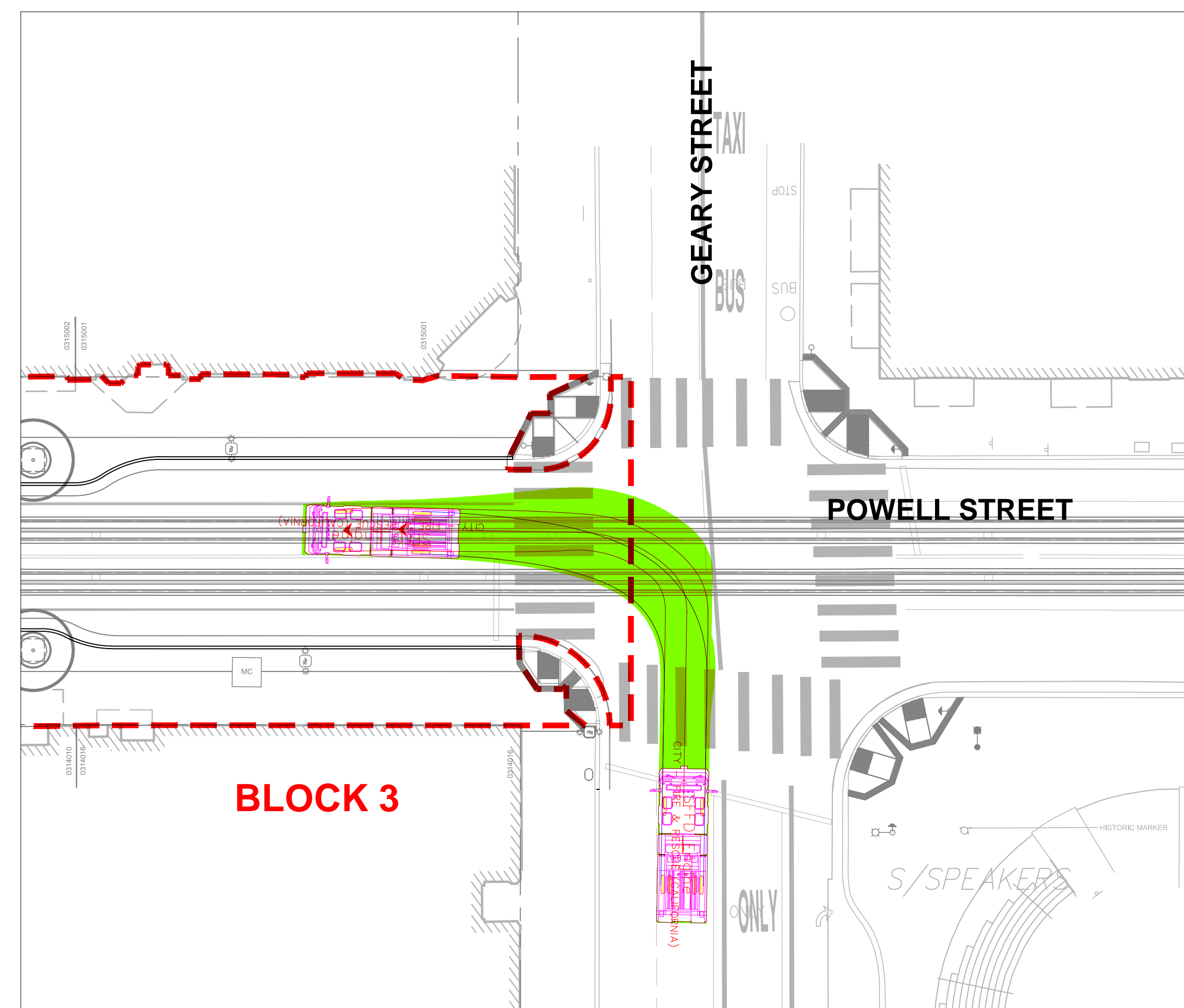
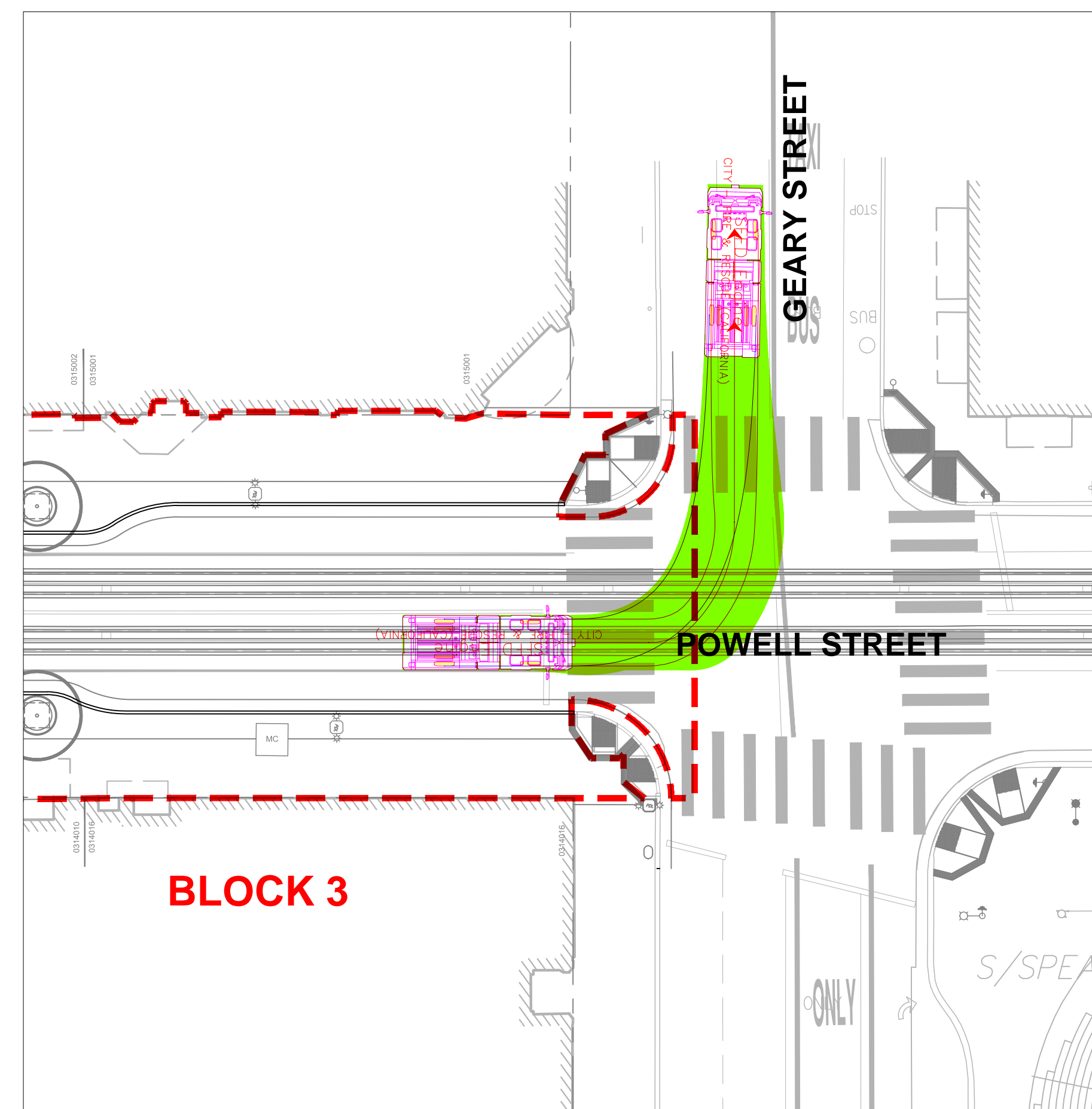
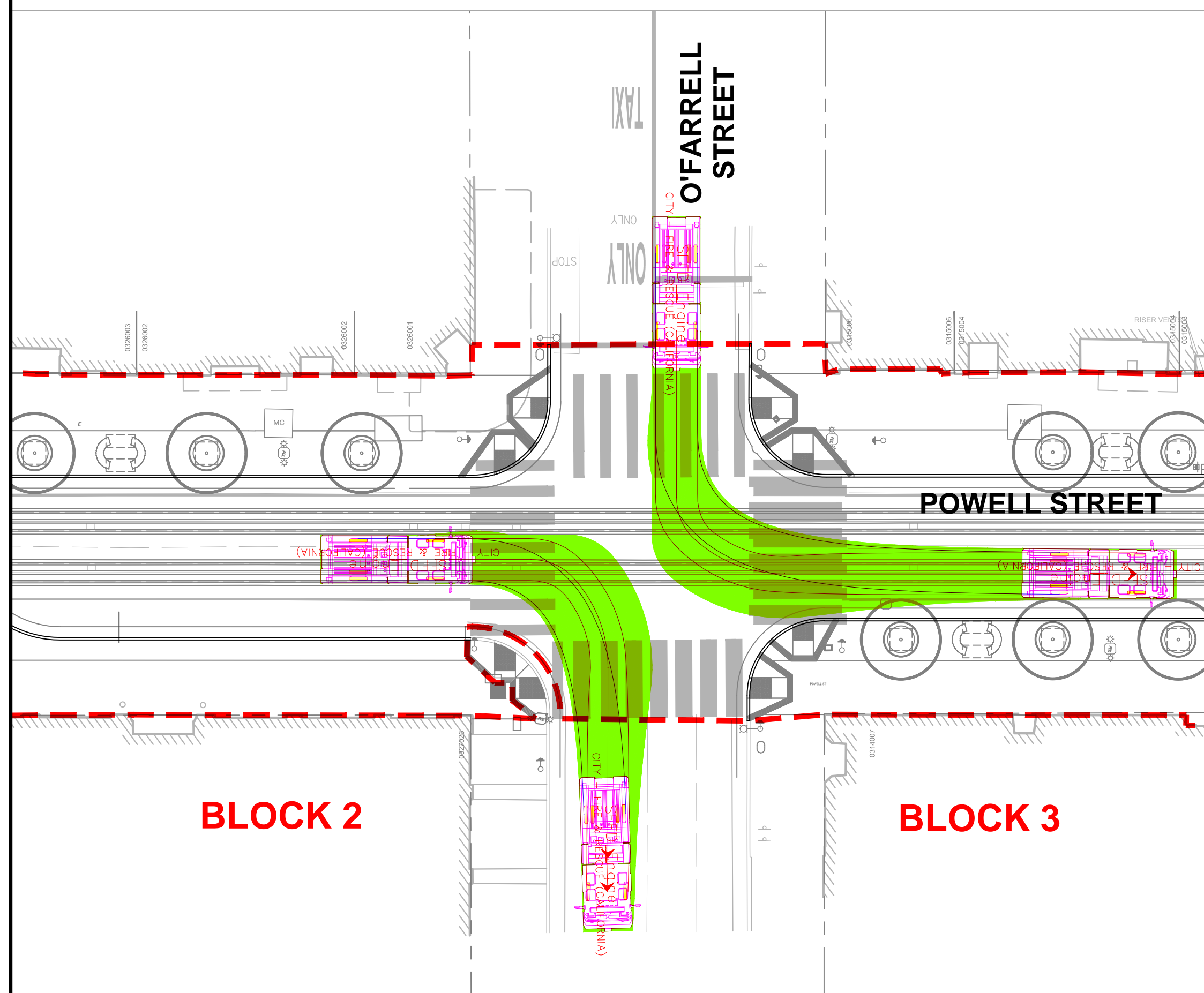
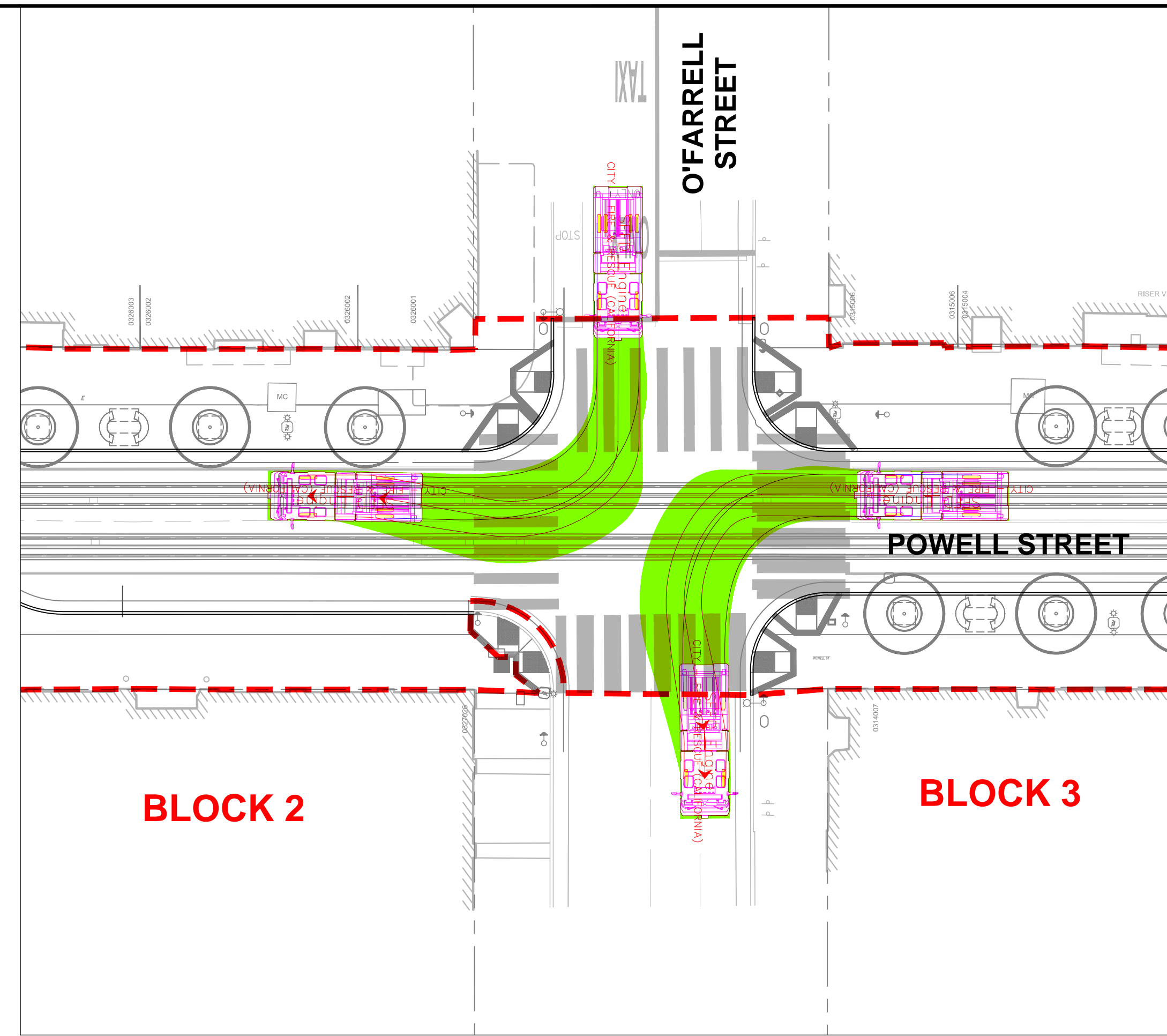
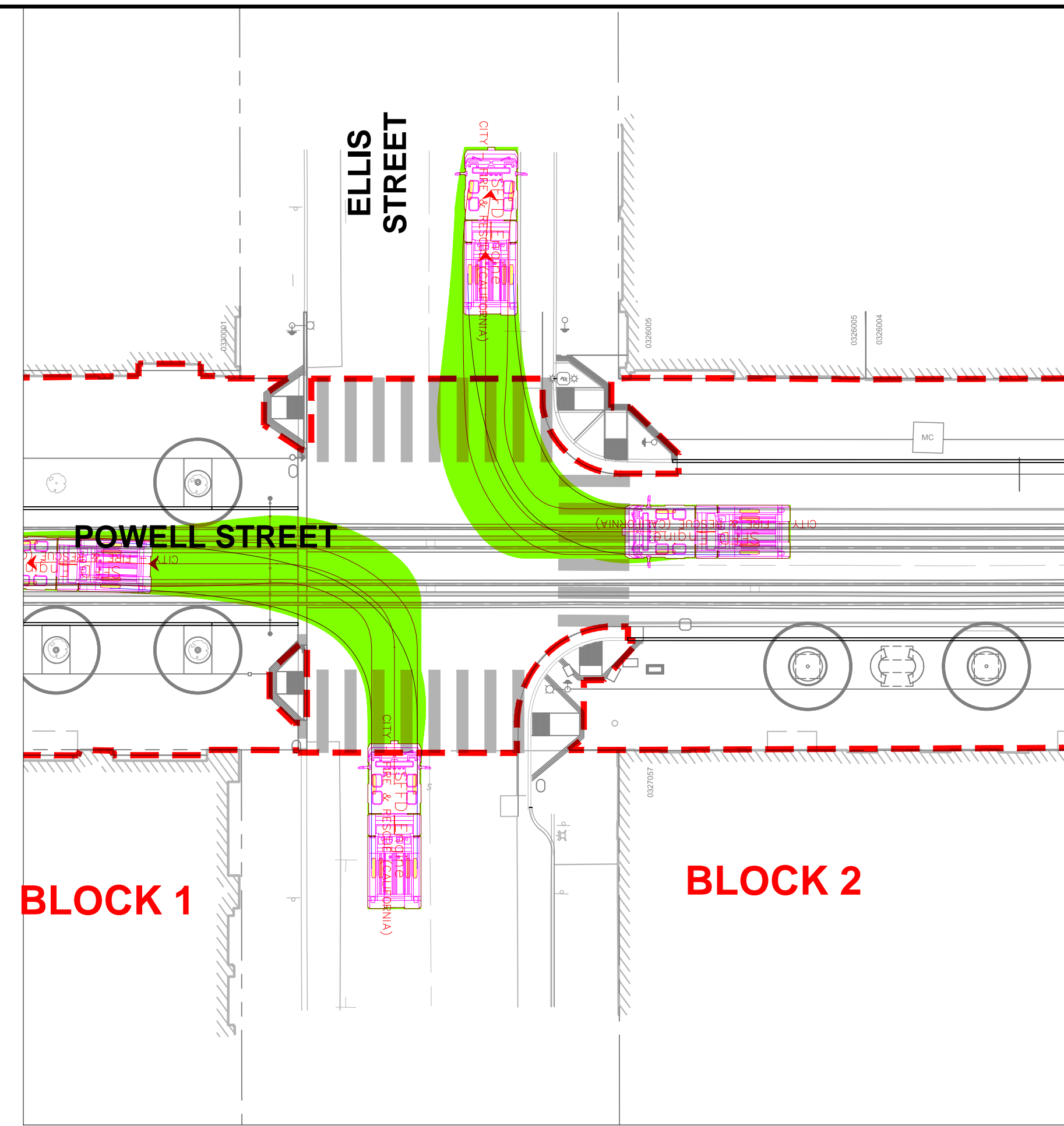
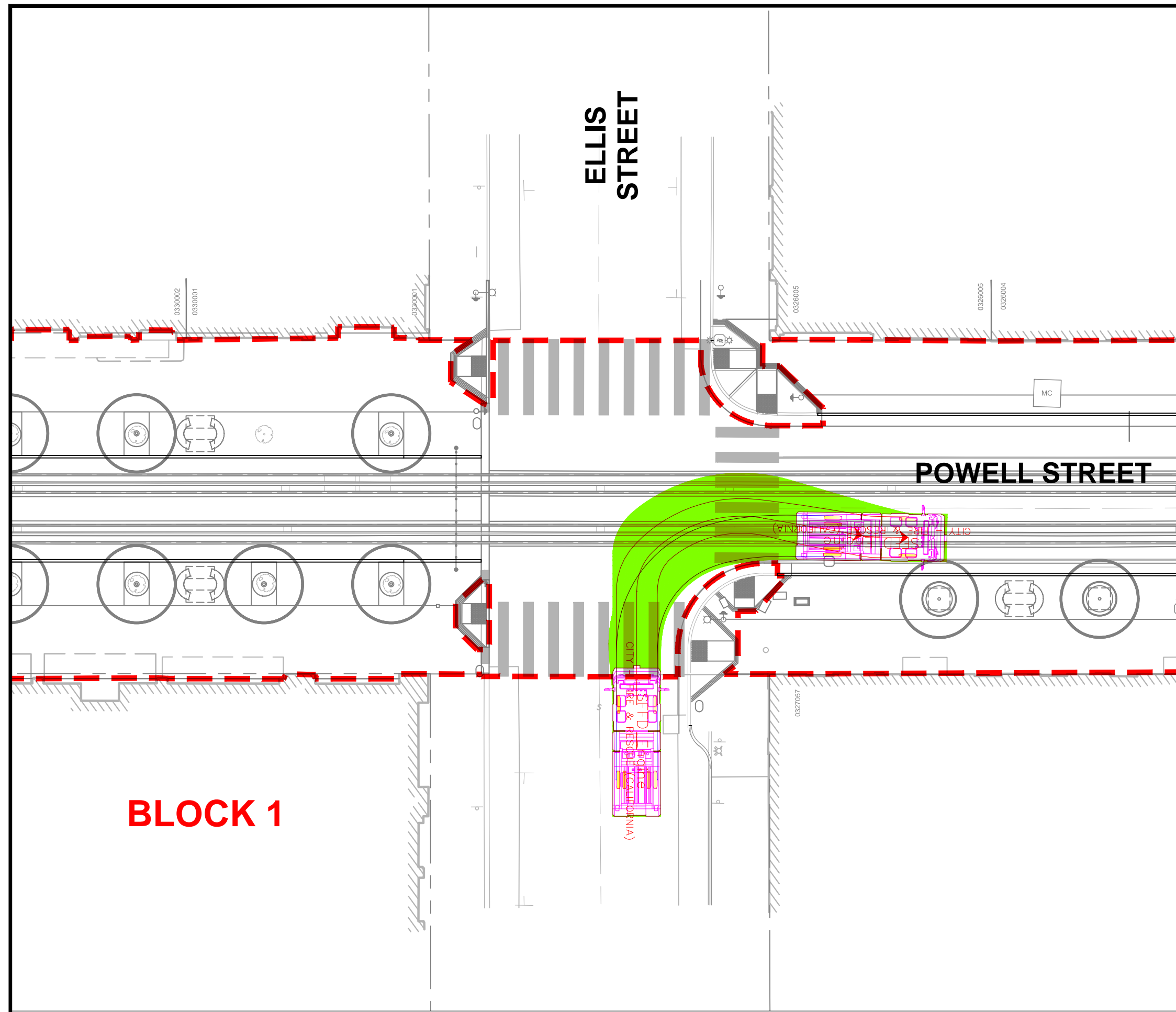


PROJECT NAME
 APN 000-000-000
 STREET NUMBER AND NAME, CITY/COUNTY
TRUCK TURNING EXHIBIT

No.	Date	Revisions

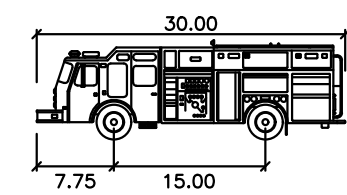
Date: AS SHOWN
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 Drawn: DRWN
 Approved: APRV
 Job No.: JOB NO
EXH 3
 3 OF 7

PKI Mar 19, 2026 at 3:15pm



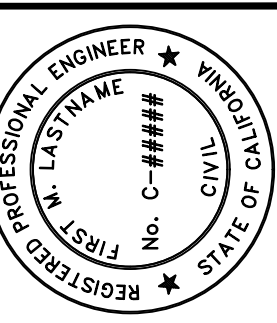
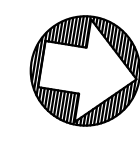
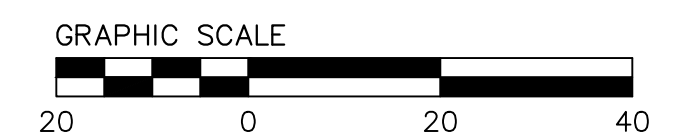
LEGEND:

- DIRECTION OF TRAVEL
- REAR TIRE EXTERIOR LIMIT
- FRONT TIRE EXTERIOR LIMIT
- VEHICLE BODY TURNING ENVELOPE



SFFD Engine feet
 Width : 9.50
 Track : 8.17
 Lock to Lock Time : 6.0
 Steering Angle : 36.0

SFFD TRUCK TURNING EXHIBIT



PRELIMINARY
 NOT FOR CONSTRUCTION

BKF ENGINEERS
 7901 STONERIDGE DRIVE
 SUITE 360
 PLEASANTON, CA 94588
 (925) 396-7700
 www.bkf.com



PROJECT NAME
 APN 000-000-000
 STREET NUMBER AND NAME, CITY/COUNTY

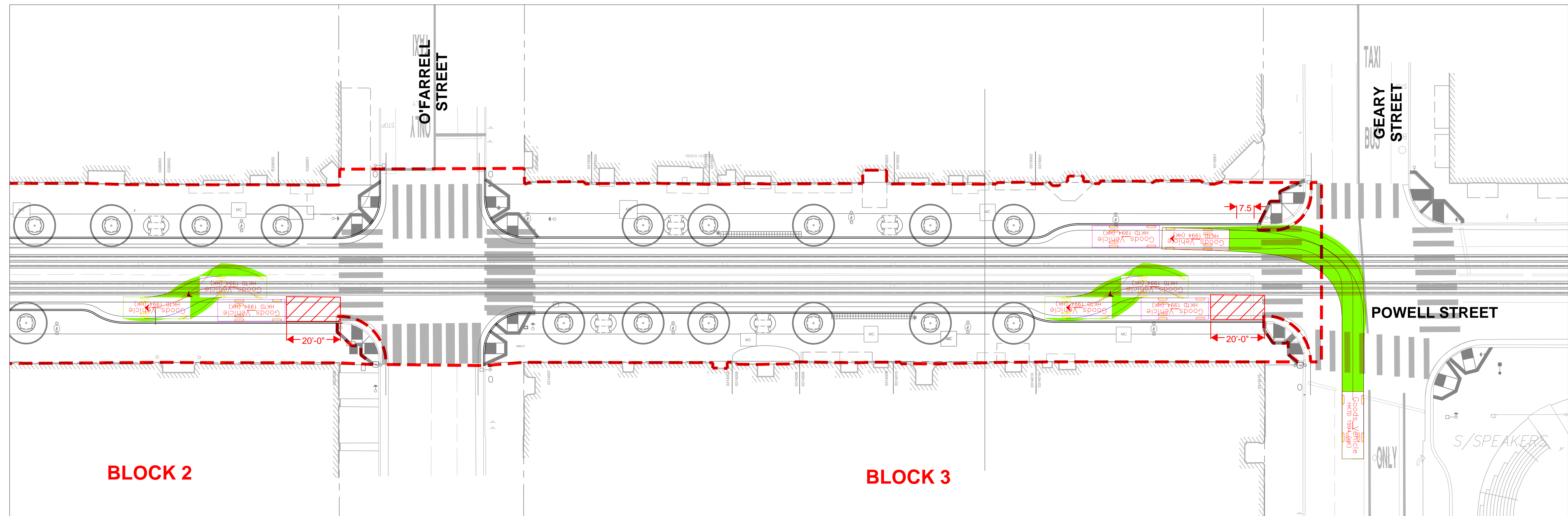
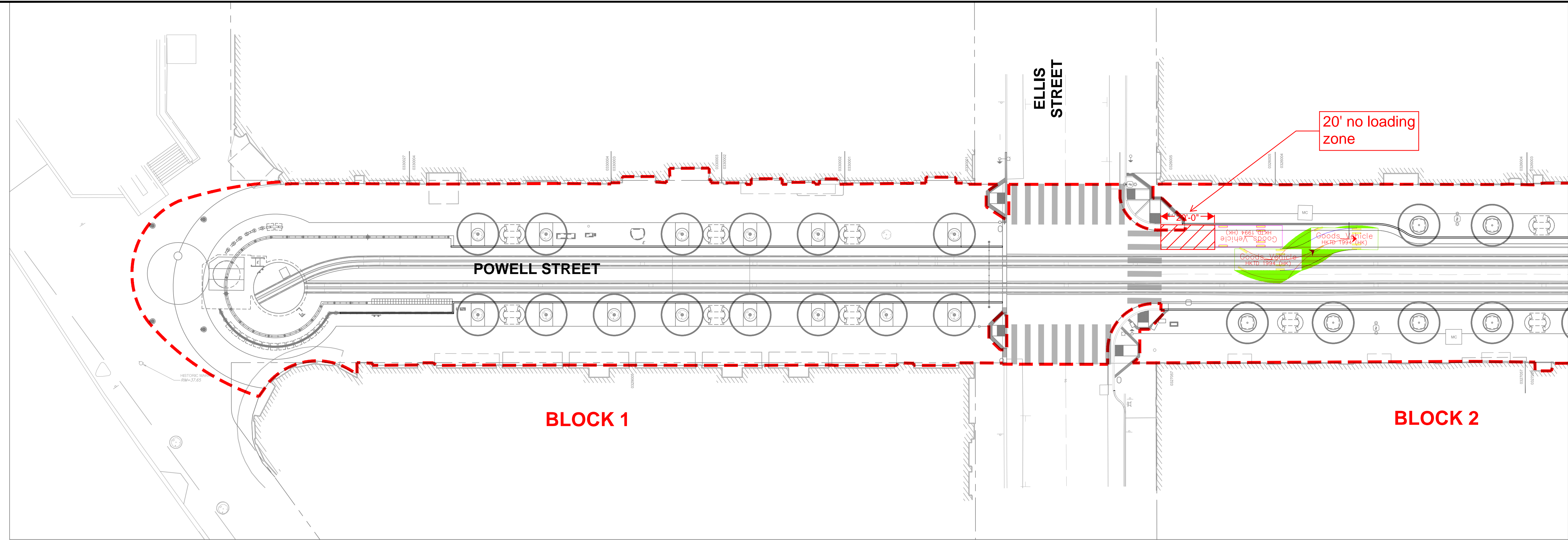
TRUCK TURNING EXHIBIT

No.	Date	Description

Drawing Number:
EXH 4
 4 OF 7





PKI Mar 19, 2026 at 3:15pm
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 COPYRIGHT © 2026, BKF ENGINEERS

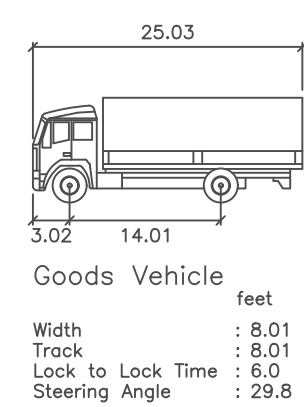
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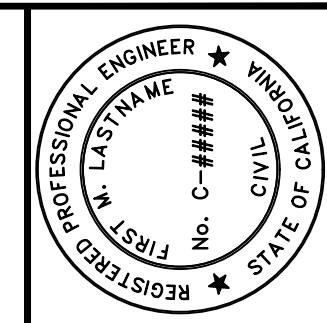
MATCH LINE, SEE BELOW

LEGEND:

-  DIRECTION OF TRAVEL
-  REAR TIRE EXTERIOR LIMIT
-  FRONT TIRE EXTERIOR LIMIT
-  VEHICLE BODY TURNING ENVELOPE



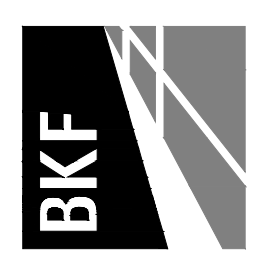
GOODS TRUCK TURNING EXHIBIT



PRELIMINARY
NOT FOR CONSTRUCTION

DATE: 03/19/2026
FIRST M. LASTNAME C-####

BKF ENGINEERS
7901 STONERIDGE DRIVE
SUITE 360
PLEASANTON, CA 94588
(925) 396-7700
www.bkf.com



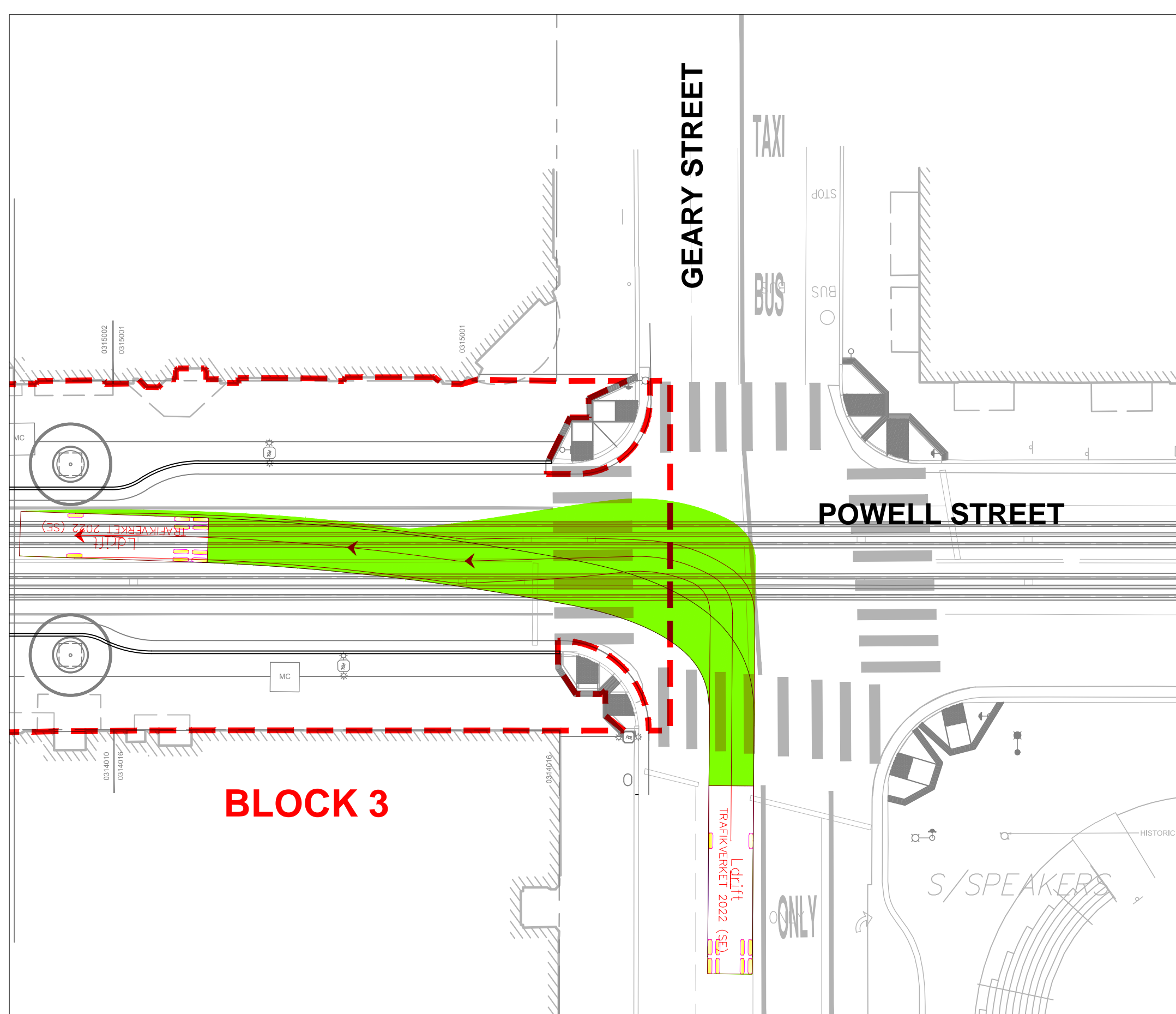
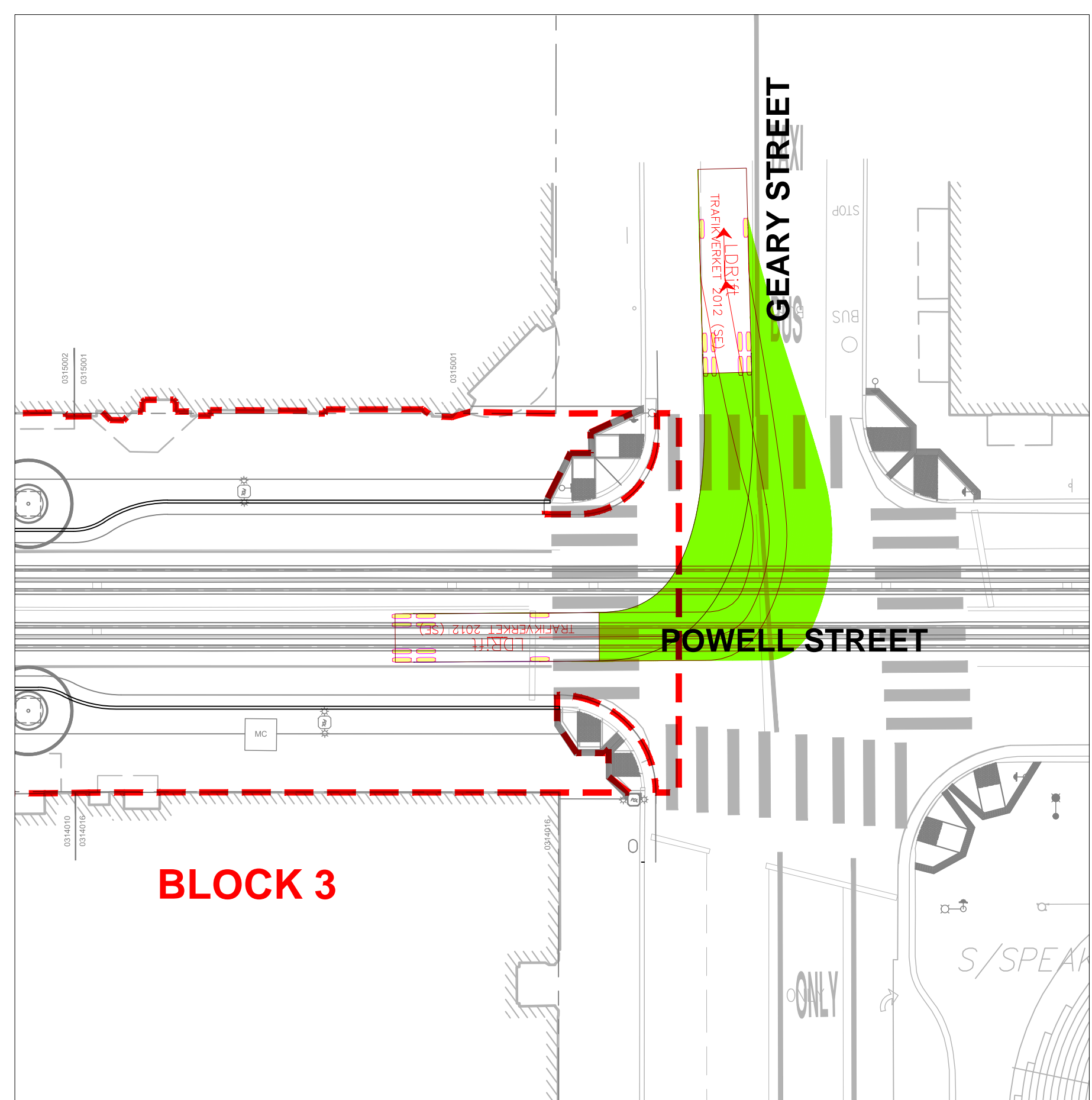
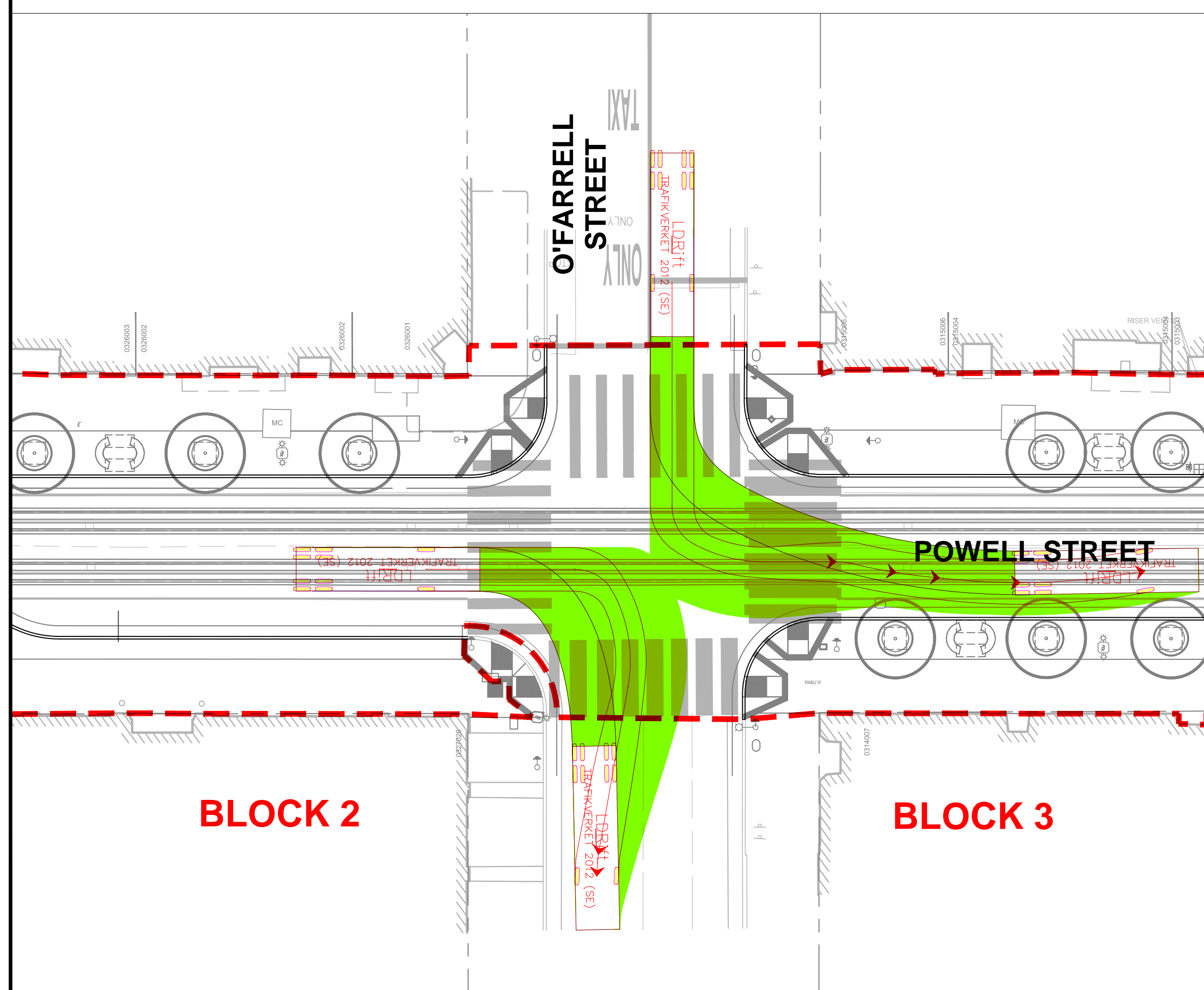
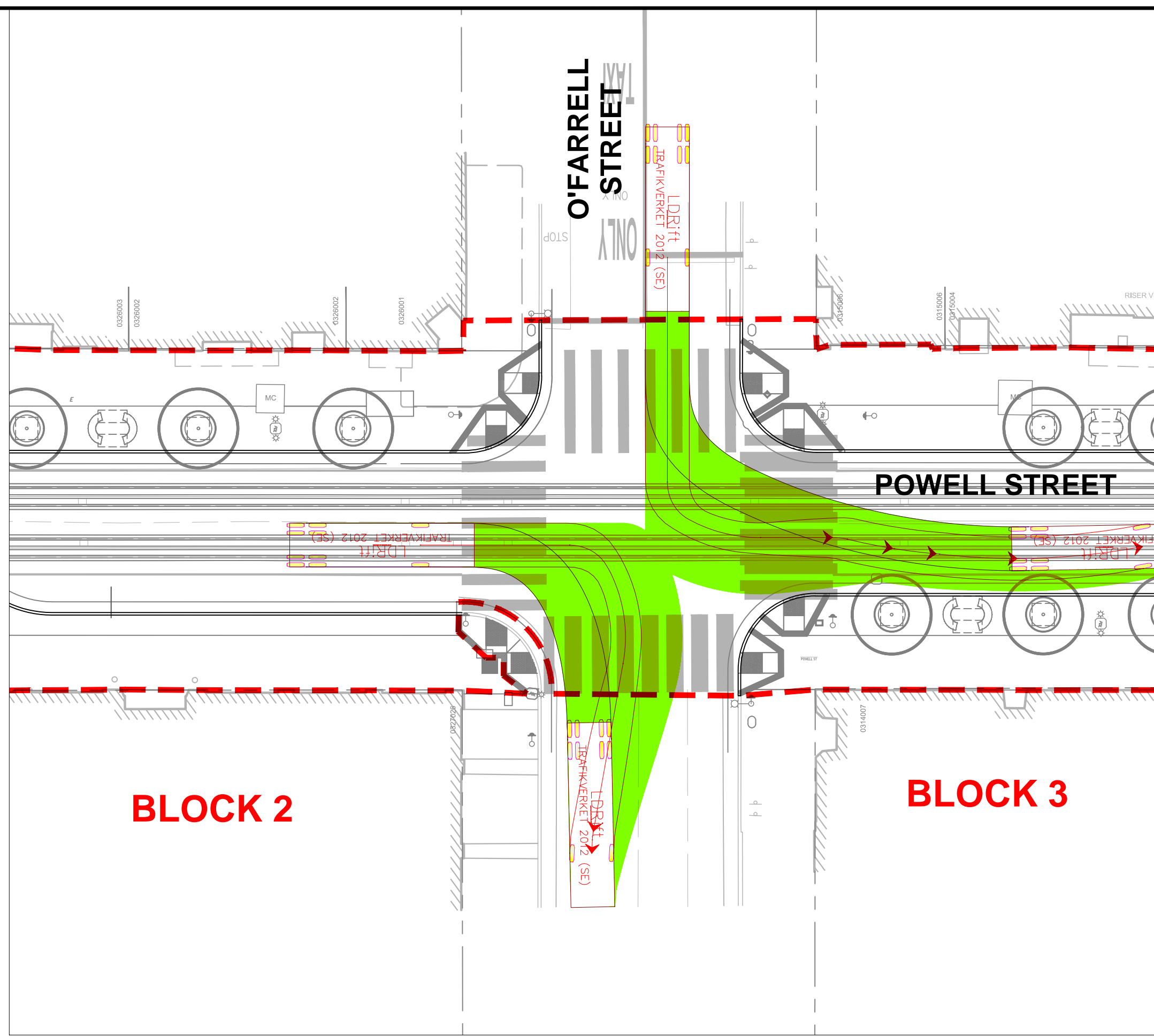
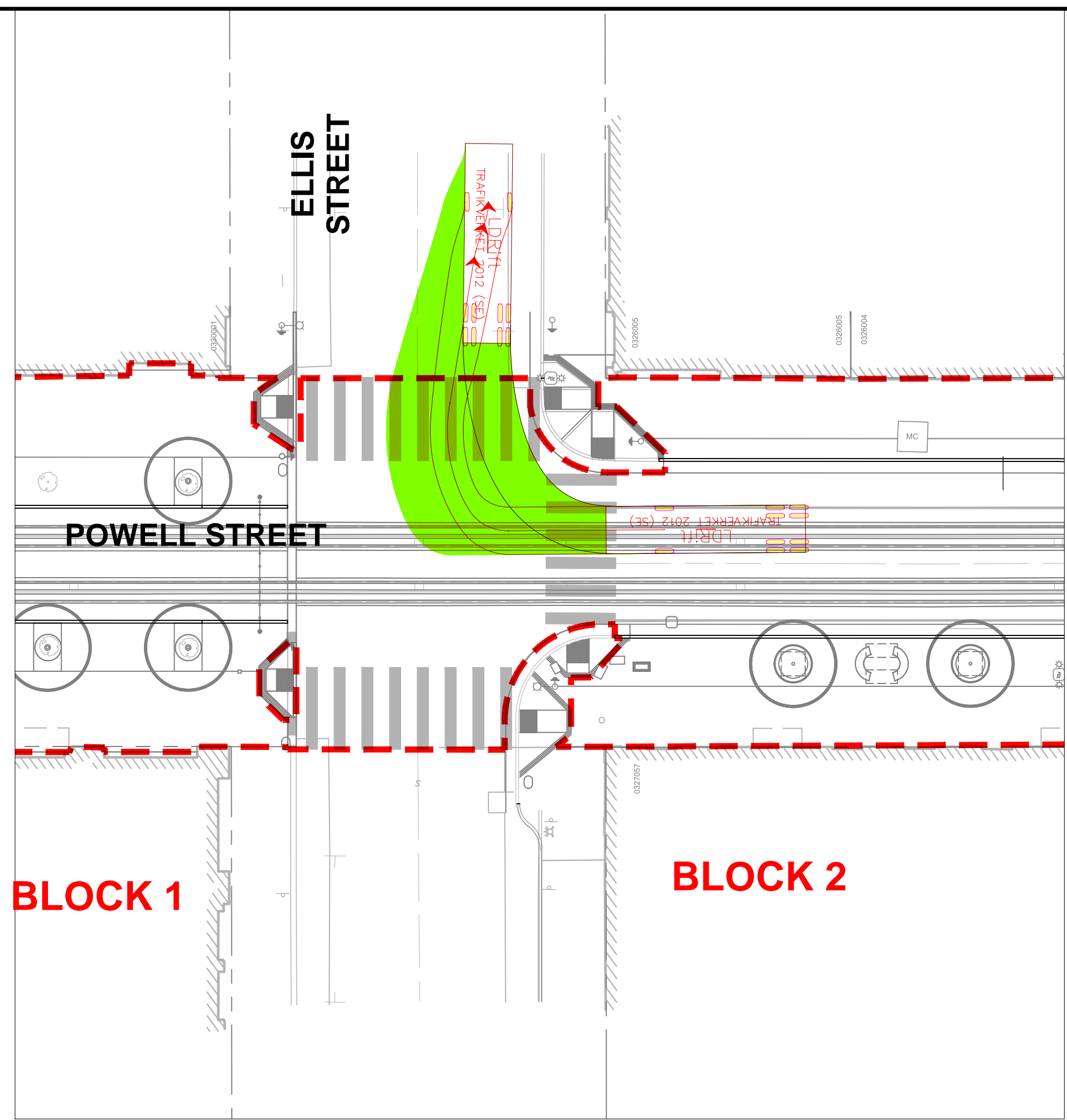
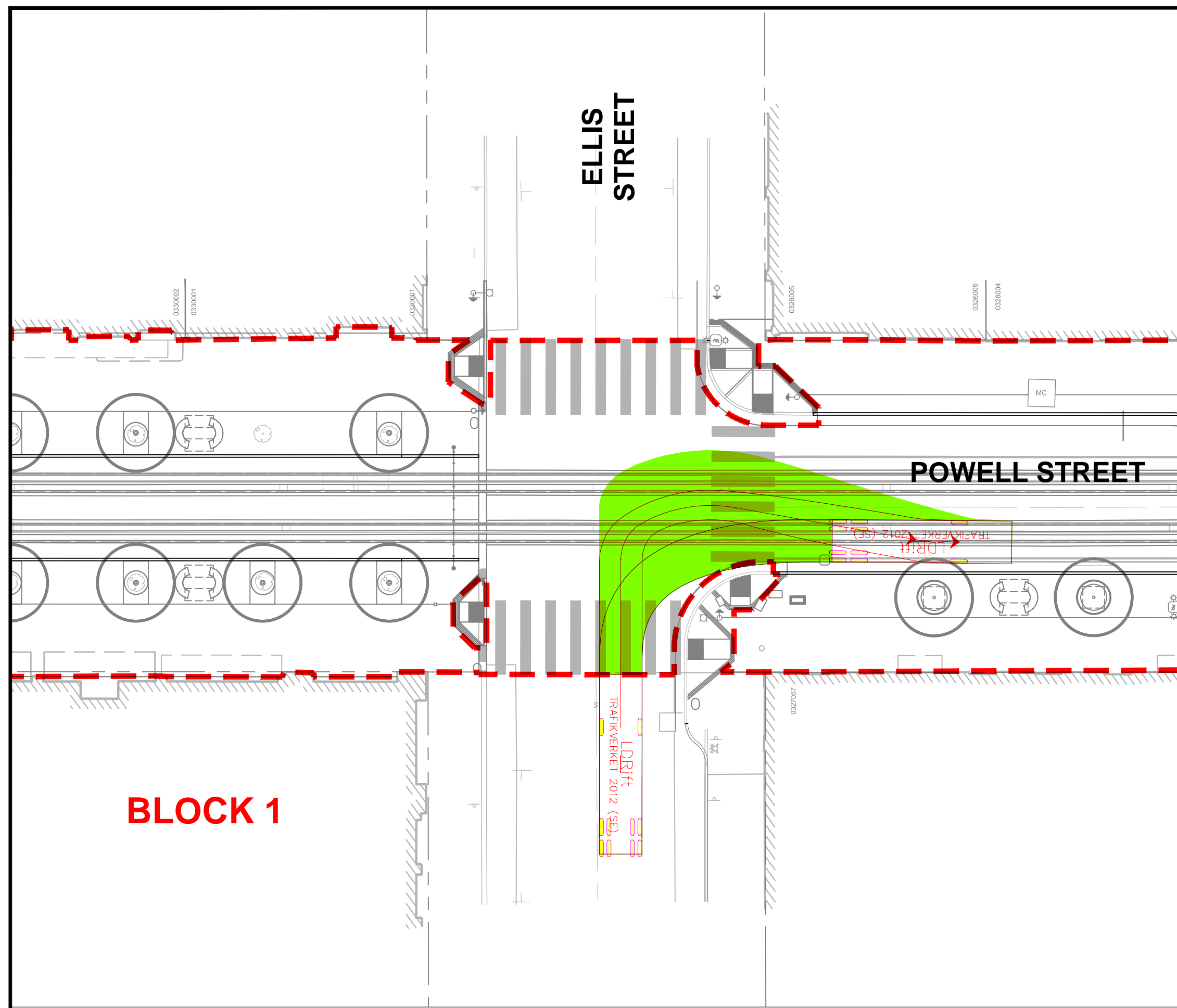
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APN 000-000-000
STREET NUMBER AND NAME, CITY/COUNTY

TRUCK TURNING EXHIBIT

No.	Date	Revisions

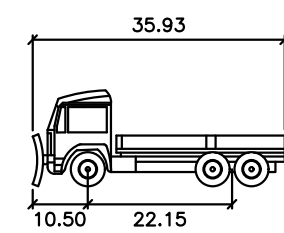
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Design: DESN
Drawn: DRWN
Approved: APRV
Job No.: JOB_NO

Drawing Number:
EXH 5
5 OF 7



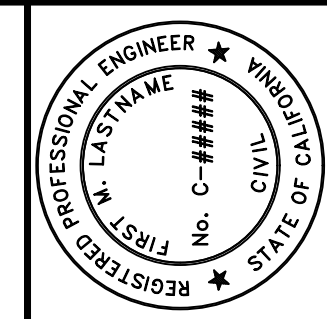
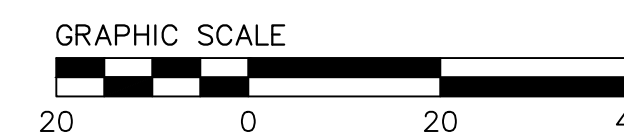
LEGEND:

- DIRECTION OF TRAVEL
- REAR TIRE EXTERIOR LIMIT
- FRONT TIRE EXTERIOR LIMIT
- VEHICLE BODY TURNING ENVELOPE



LDRift
 feet
 Width : 8.53
 Track : 8.53
 Lock to Lock Time : 6.0
 Steering Angle : 51.3

TOW TRUCK TURNING EXHIBIT



PRELIMINARY
 NOT FOR CONSTRUCTION

DATE: 03/19/2026
 FIRST M. LASTNAME C-####

BKF ENGINEERS
 7901 STORERIDGE DRIVE
 SUITE 360
 PLEASANTON, CA 94588
 (925) 396-7700
 www.bkf.com



PROJECT NAME
 APN 000-000-000

STREET NUMBER AND NAME, CITY/COUNTY

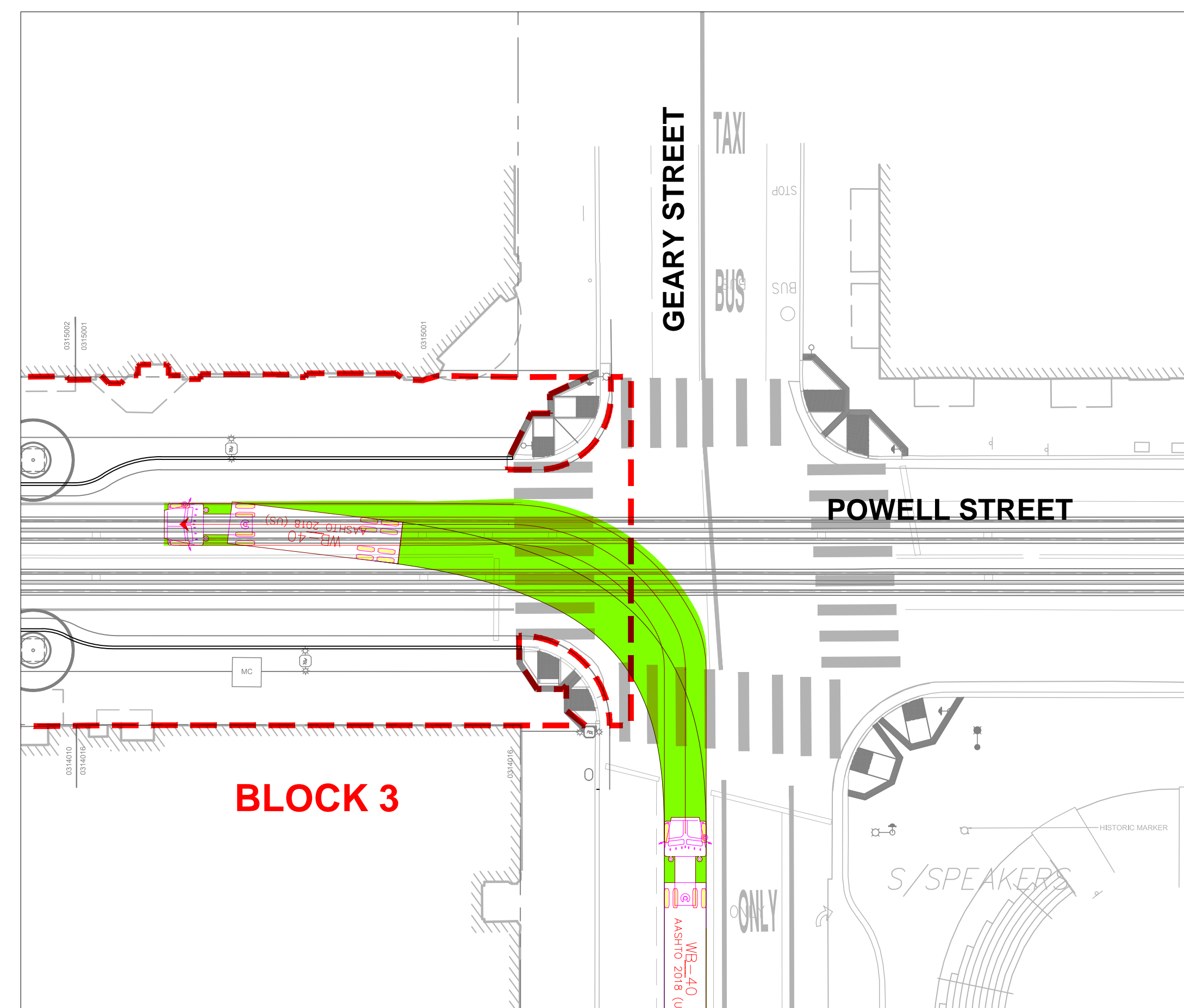
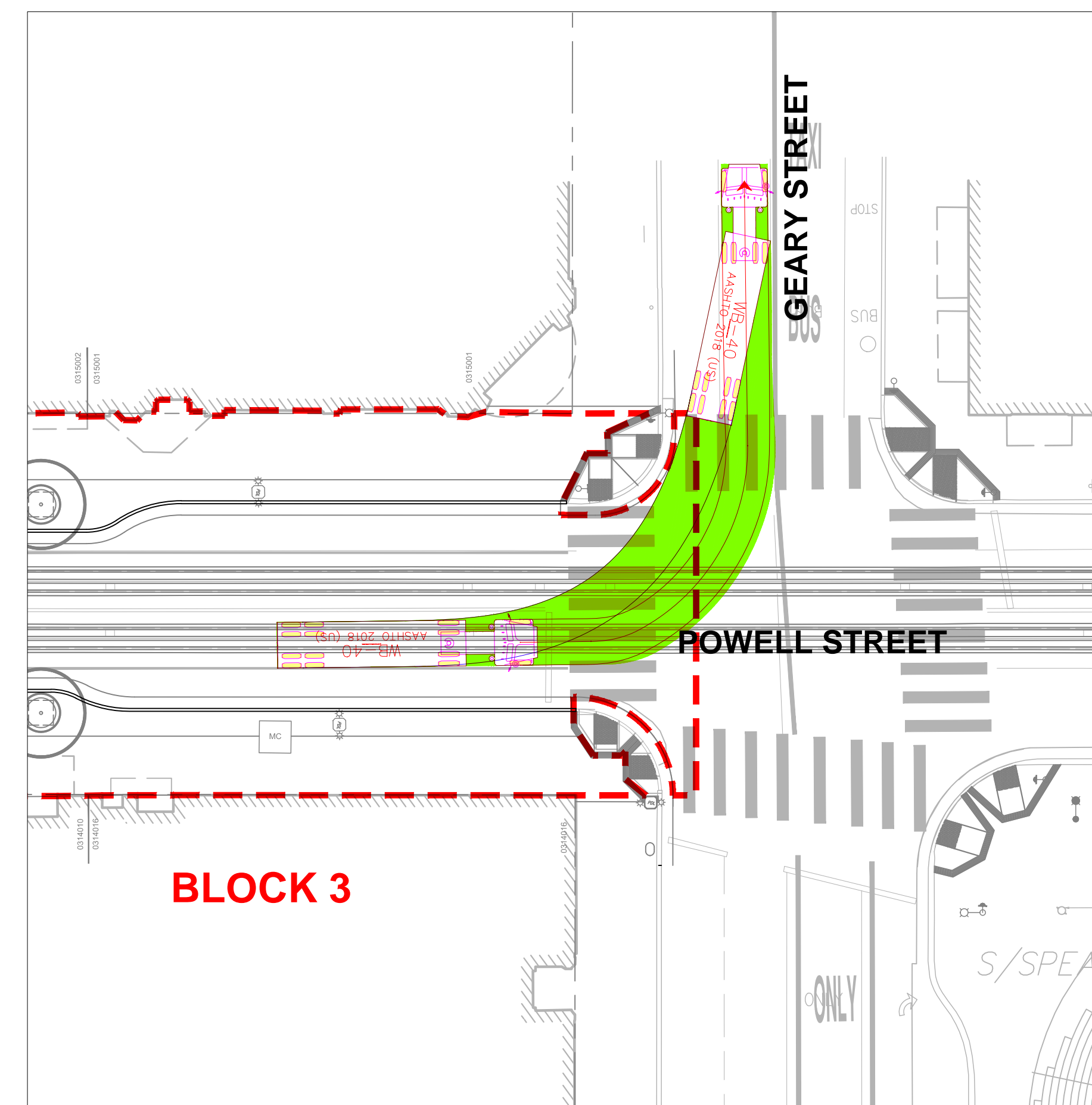
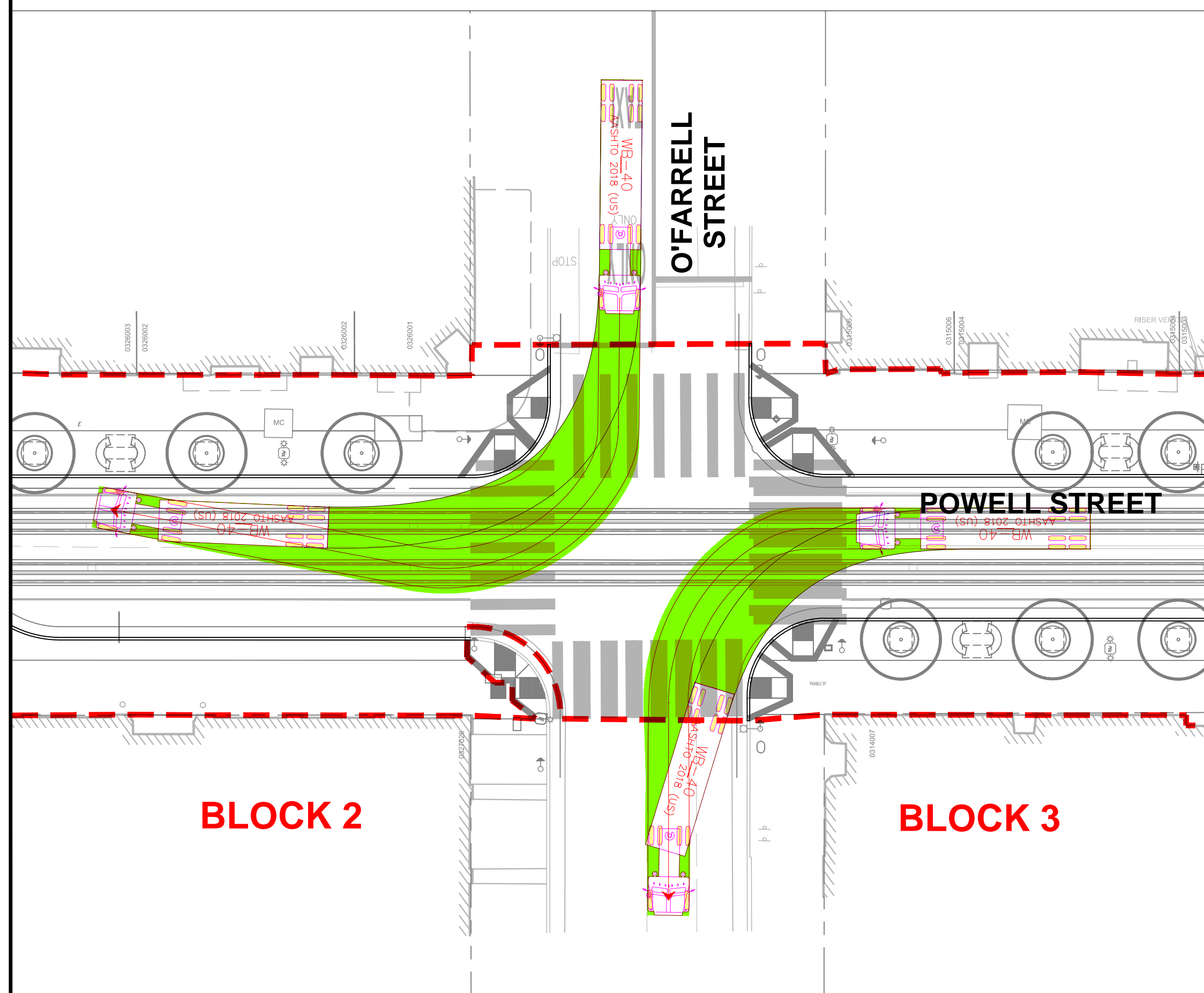
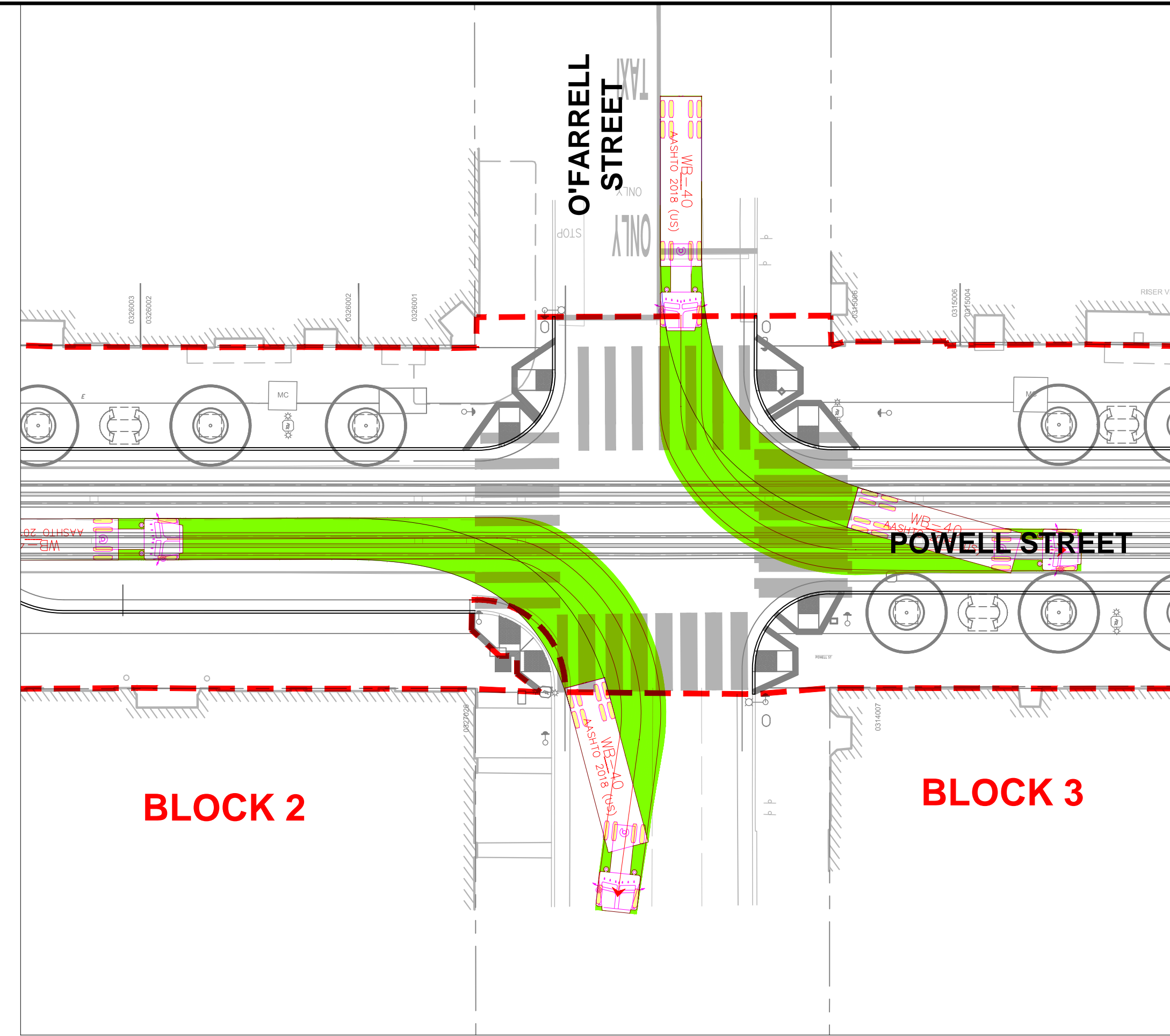
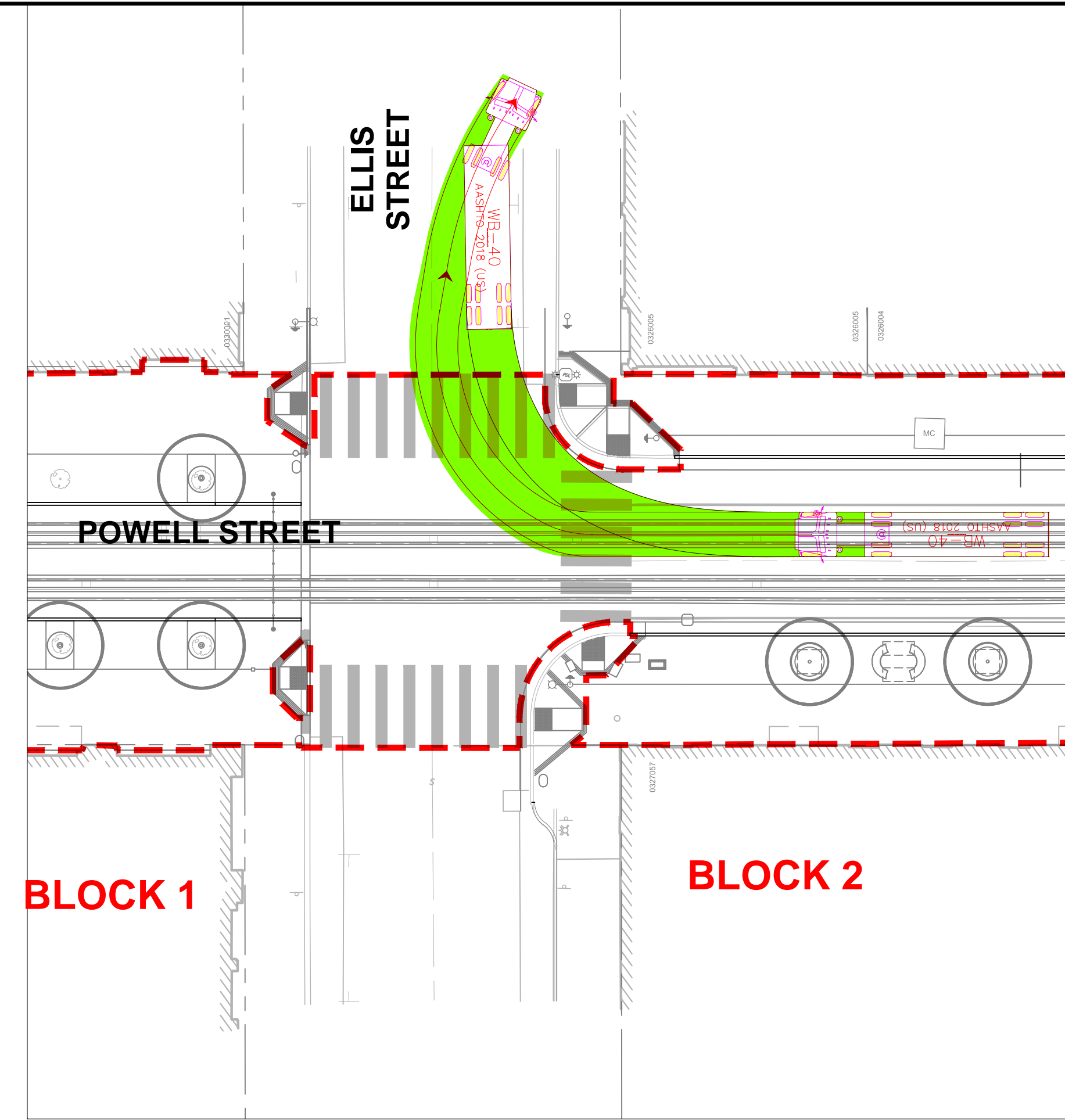
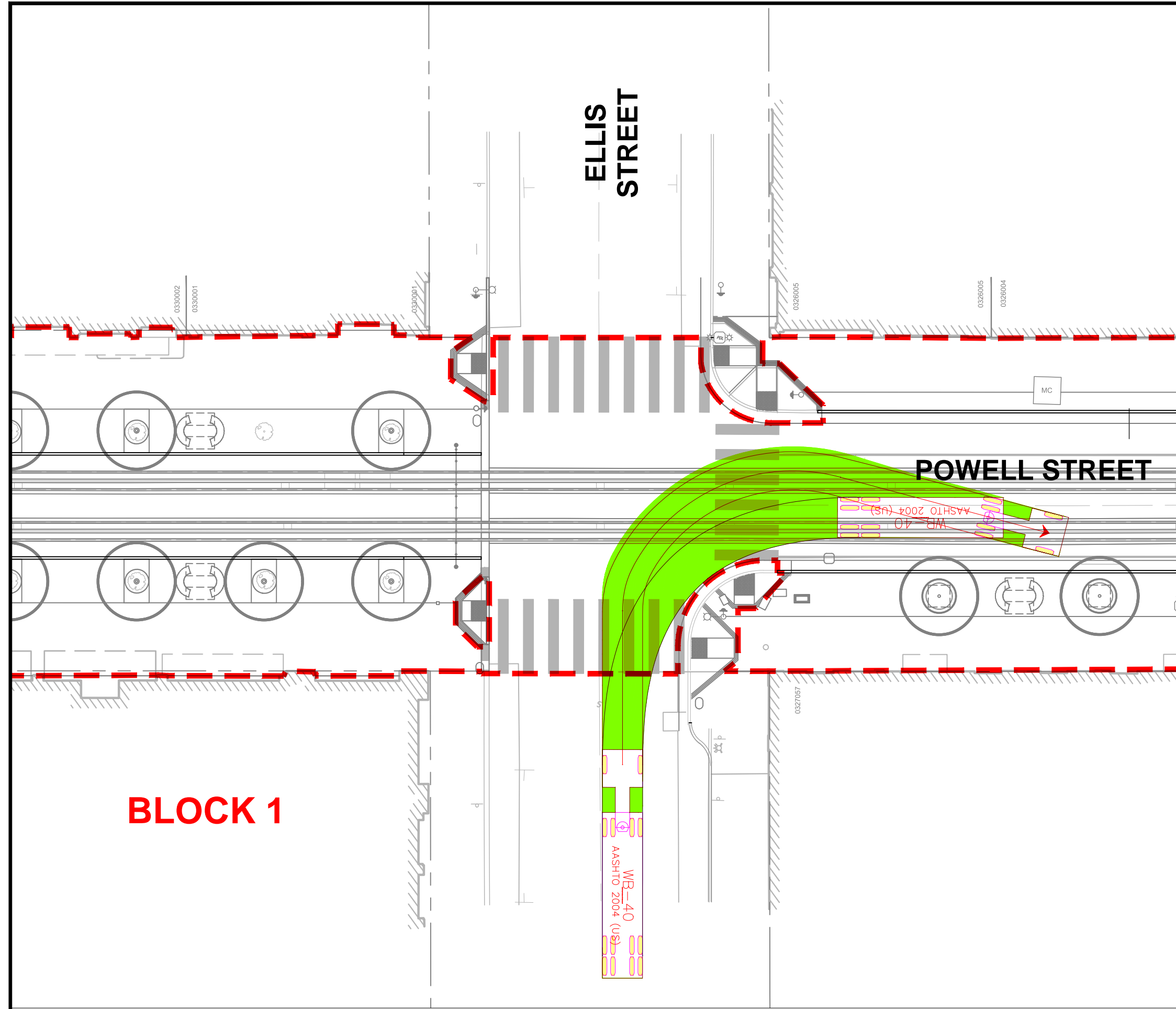
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No.	Date	Description

Date: DATE
 Scale: AS SHOWN
 Design: DESN
 Drawn: DRWN
 Approved: APRV
 Job No.: JOB NO

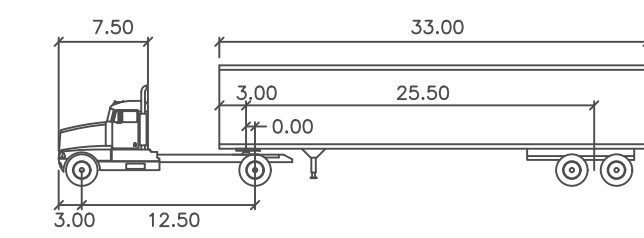
EXH 6

6 OF 7



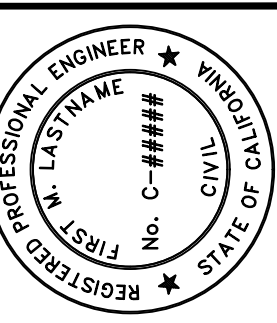
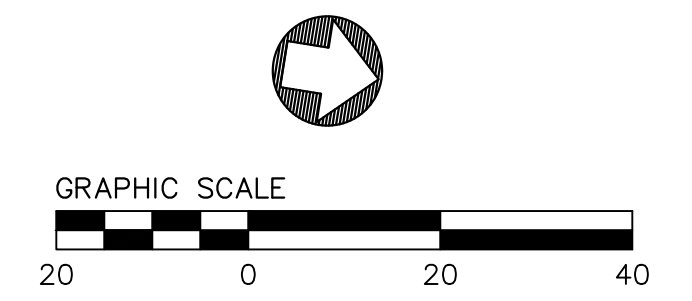
LEGEND:

- DIRECTION OF TRAVEL
- REAR TIRE EXTERIOR LIMIT
- FRONT TIRE EXTERIOR LIMIT
- VEHICLE BODY TURNING ENVELOPE



WB-40		feet	
Tractor Width	: 7.50	Lock to Lock Time	: 6.0
Trailer Width	: 33.00	Steering Angle	: 20.3
Tractor Track	: 3.00	Articulating Angle	: 70.0
Trailer Track	: 25.50		

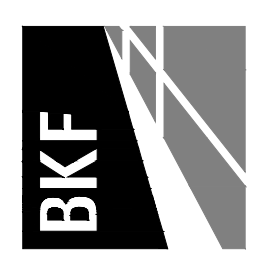
WB-40 TRUCK TURNING EXHIBIT



PRELIMINARY
NOT FOR CONSTRUCTION

DATE: 03/19/2026
FIRST M. LASTNAME C-11111

BKF ENGINEERS
7901 STONERIDGE DRIVE
SUITE 360
PLEASANTON, CA 94588
(925) 396-7700
www.bkf.com



PROJECT NAME
APN 000-000-000
STREET NUMBER AND NAME, CITY/COUNTY

TRUCK TURNING EXHIBIT

No.	Date	Description

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Scale: AS SHOWN
Design: DESN
Drawn: DRWN
Approved: APRV
Job No.: JOB NO

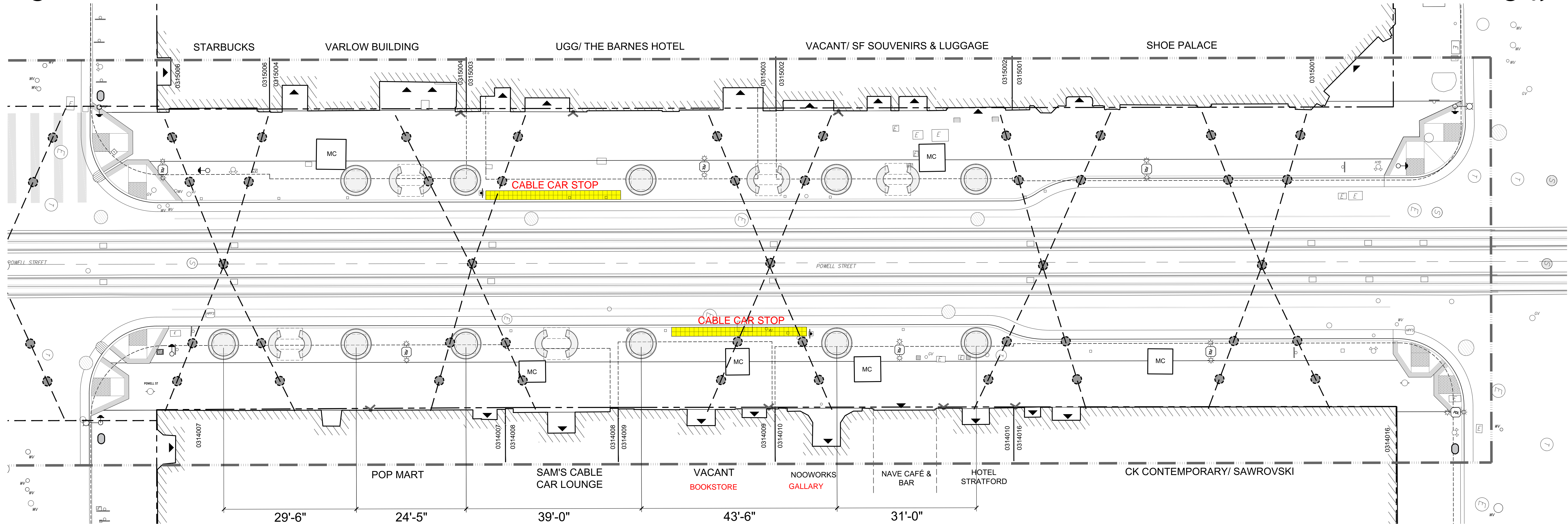
Drawing Number:
EXH 7
7 OF 7

PKI Mar 19, 2026 at 3:15pm
232516_PSPR_DD_Exhibit.dwg
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O'FARRELL STREET

Proposed midblock Cable Car Stop Locations As of 3/12/2026

GEARY STREET



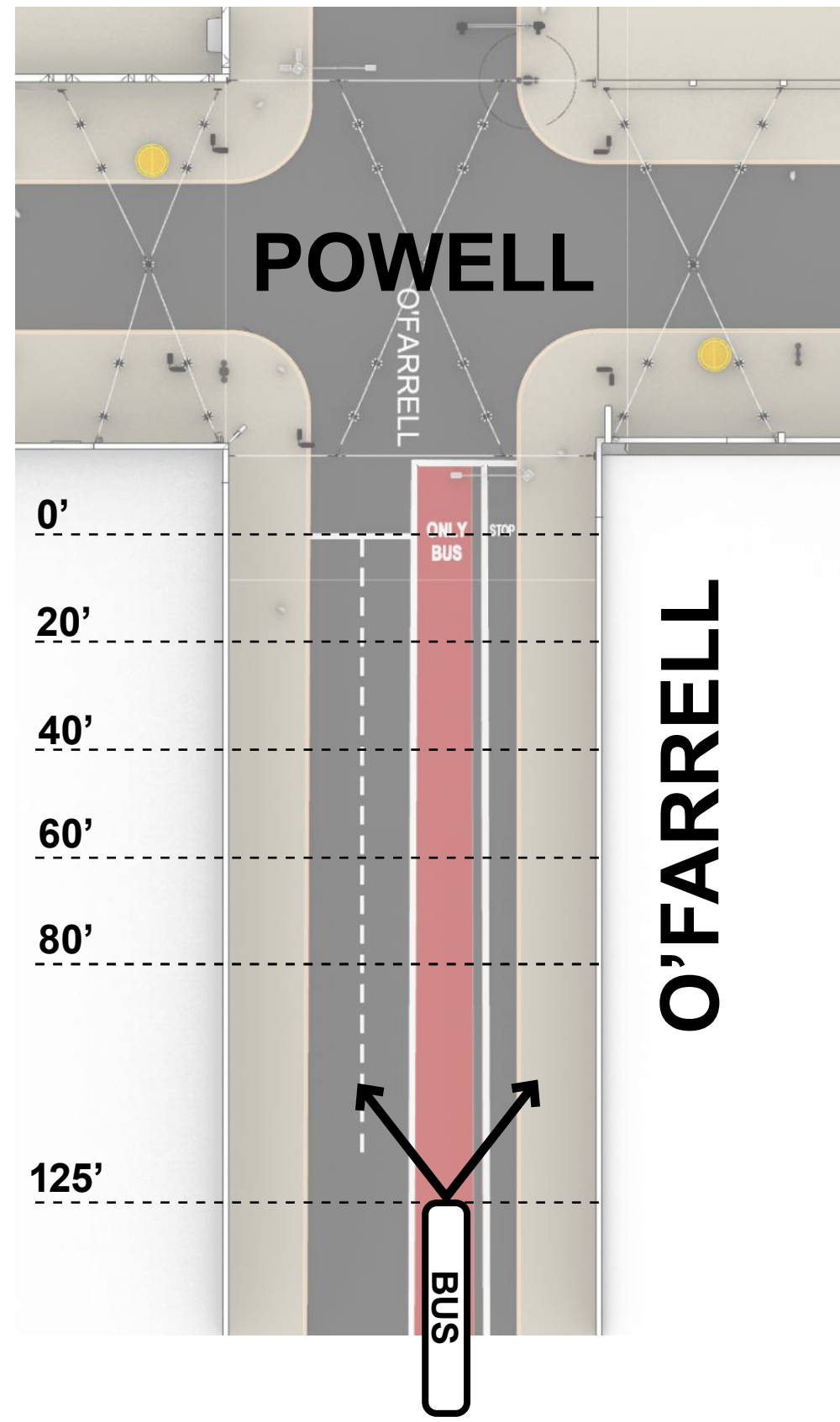
OPT-2 (PREFERRED OPTION)
Cable Car Stop Study

Centenary cable tautness, which will dictate final elevation, is under structural review.

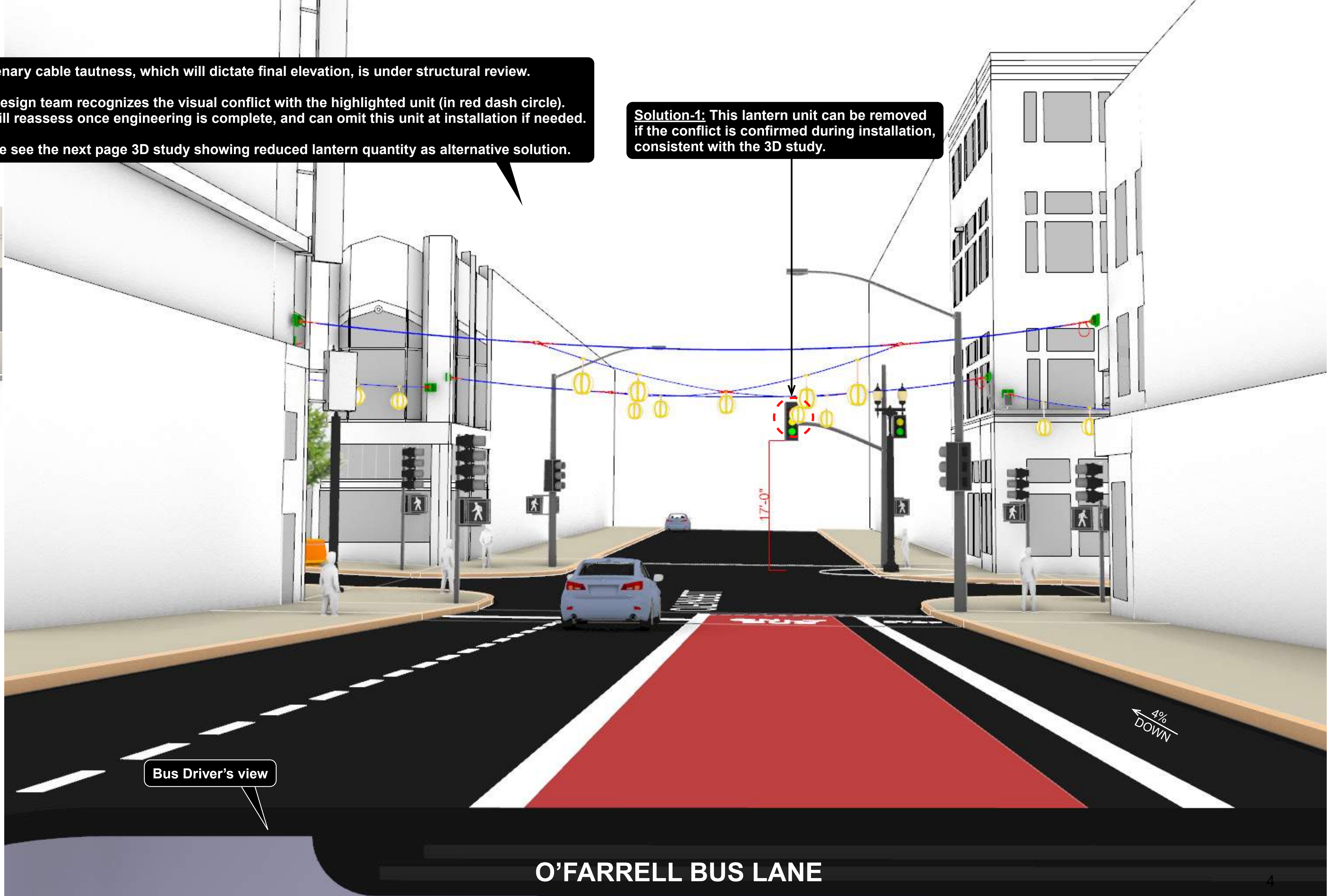
The design team recognizes the visual conflict with the highlighted unit (in red dash circle). We will reassess once engineering is complete, and can omit this unit at installation if needed.

Please see the next page 3D study showing reduced lantern quantity as alternative solution.

Solution-1: This lantern unit can be removed if the conflict is confirmed during installation, consistent with the 3D study.

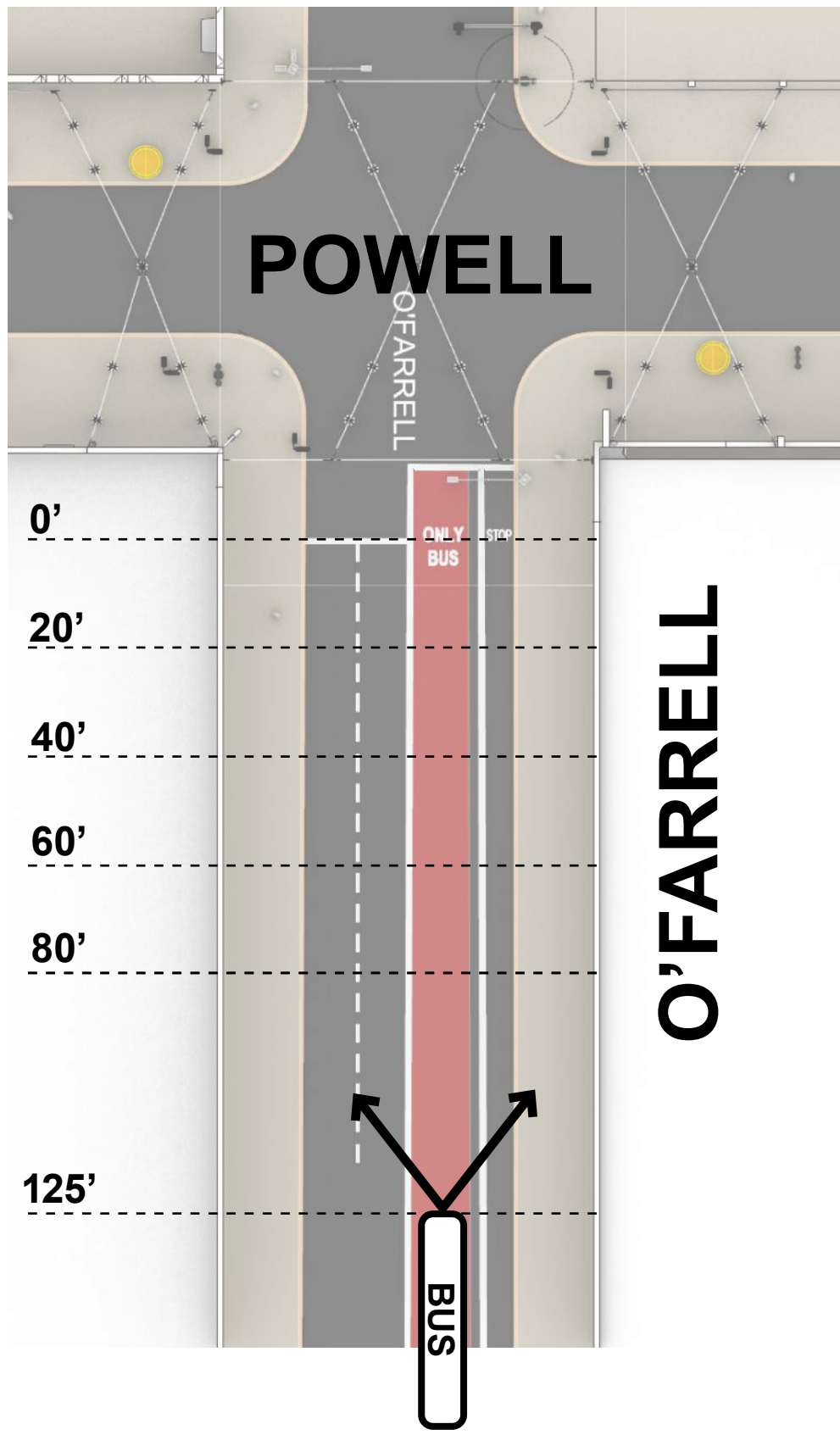


Visual Length



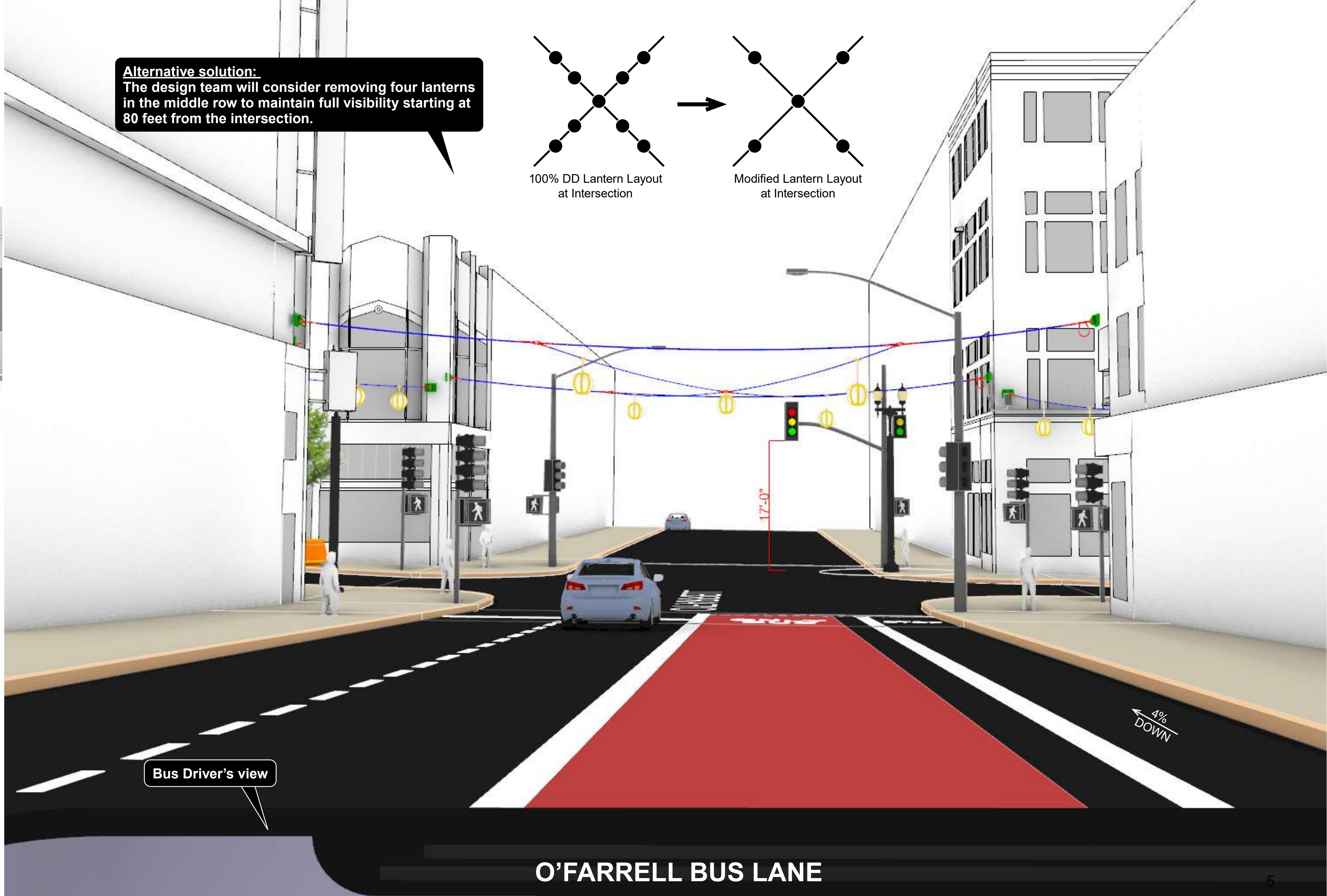
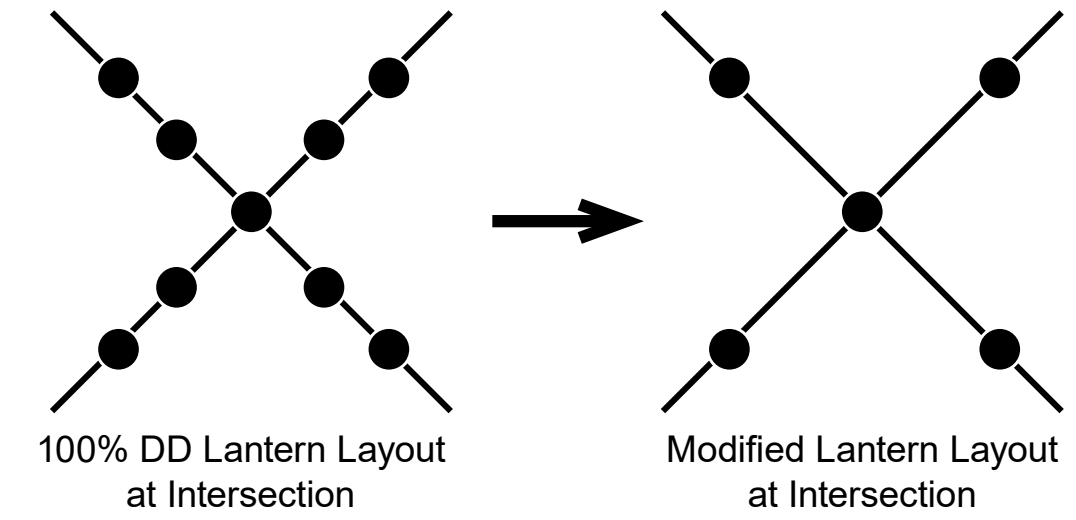
Bus Driver's view

O'FARRELL BUS LANE



Visual Length

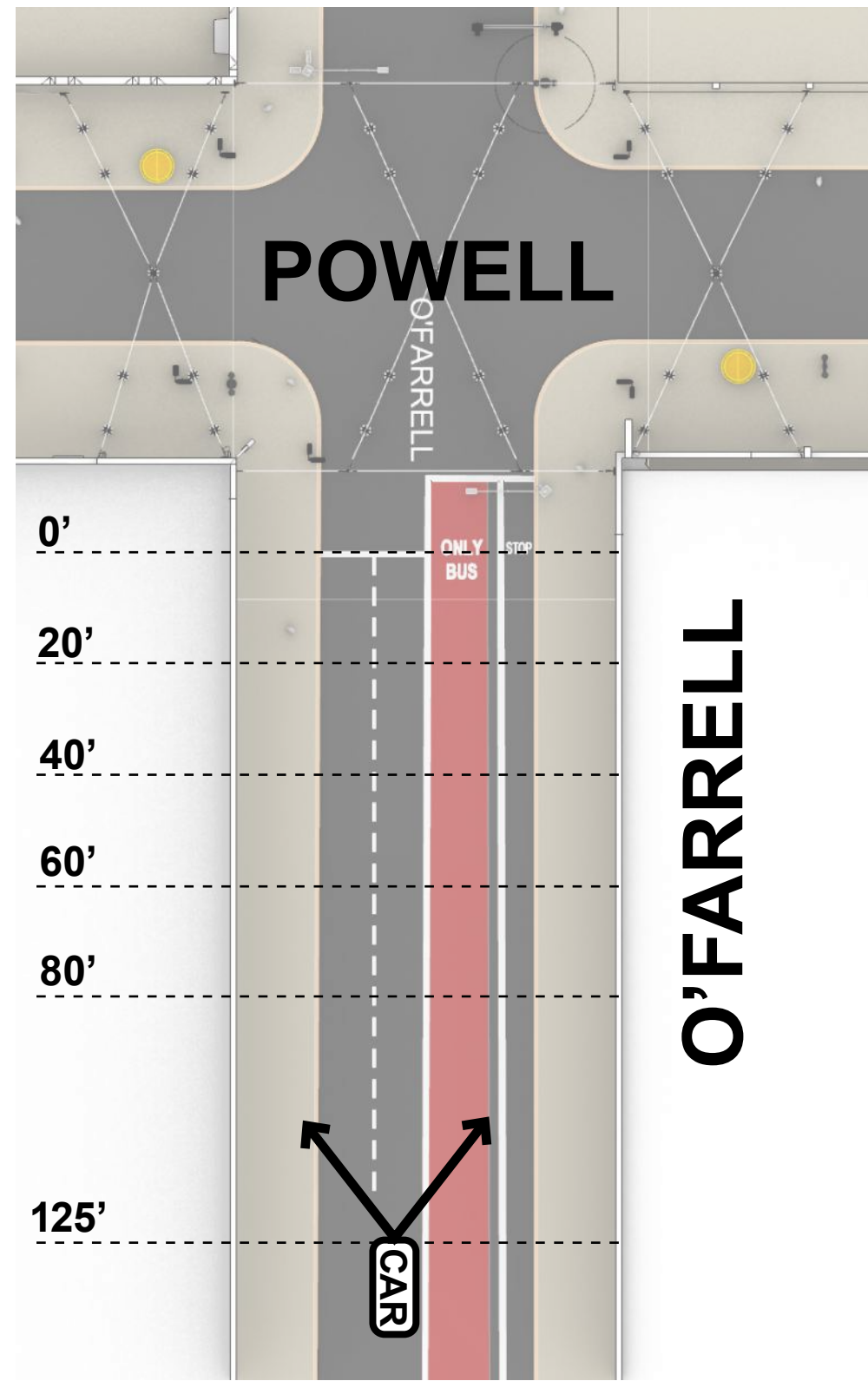
Alternative solution:
 The design team will consider removing four lanterns in the middle row to maintain full visibility starting at 80 feet from the intersection.



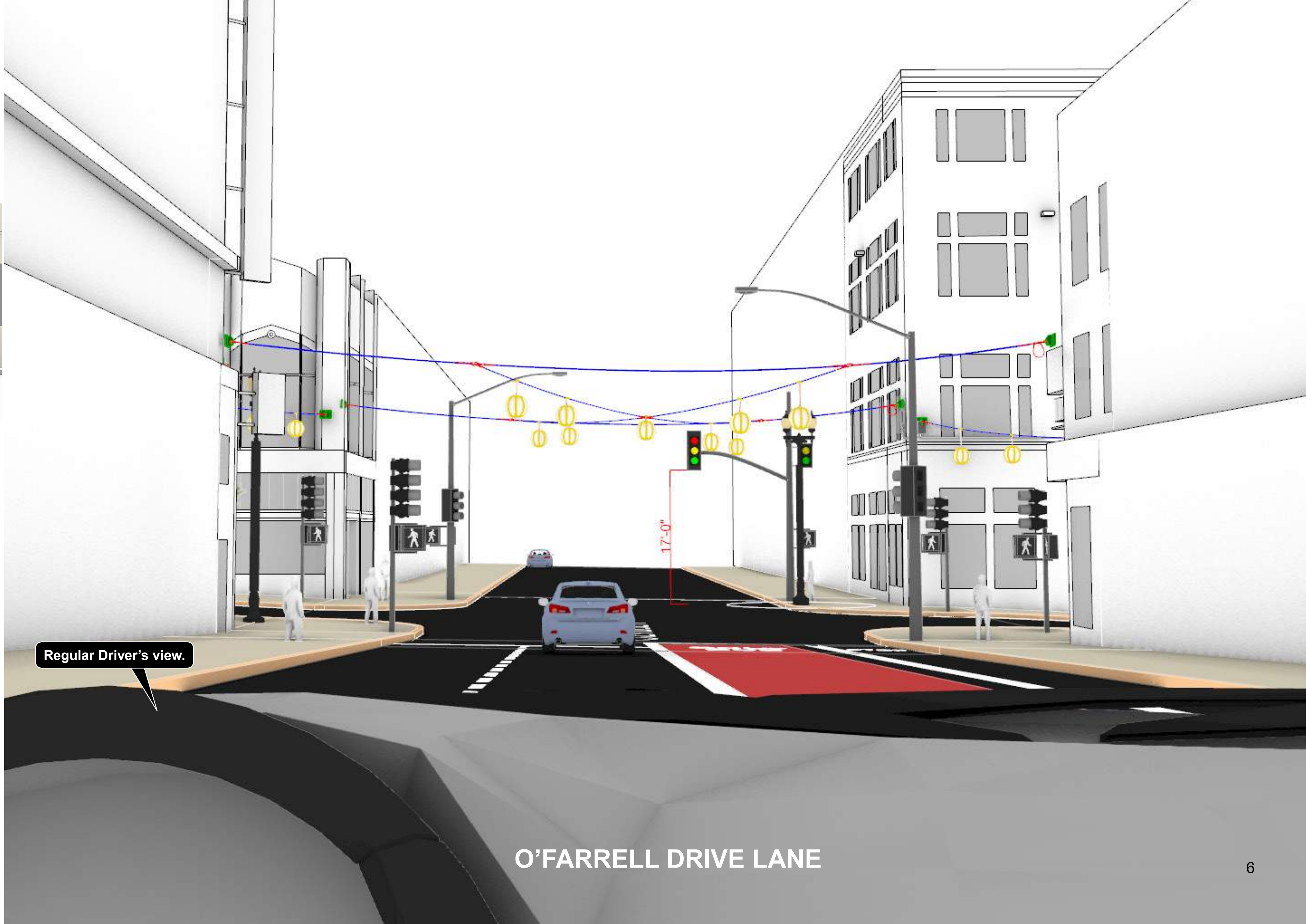
Bus Driver's view

O'FARRELL BUS LANE

Per the 3D study, on regular drive lane, the traffic signal is fully visible.



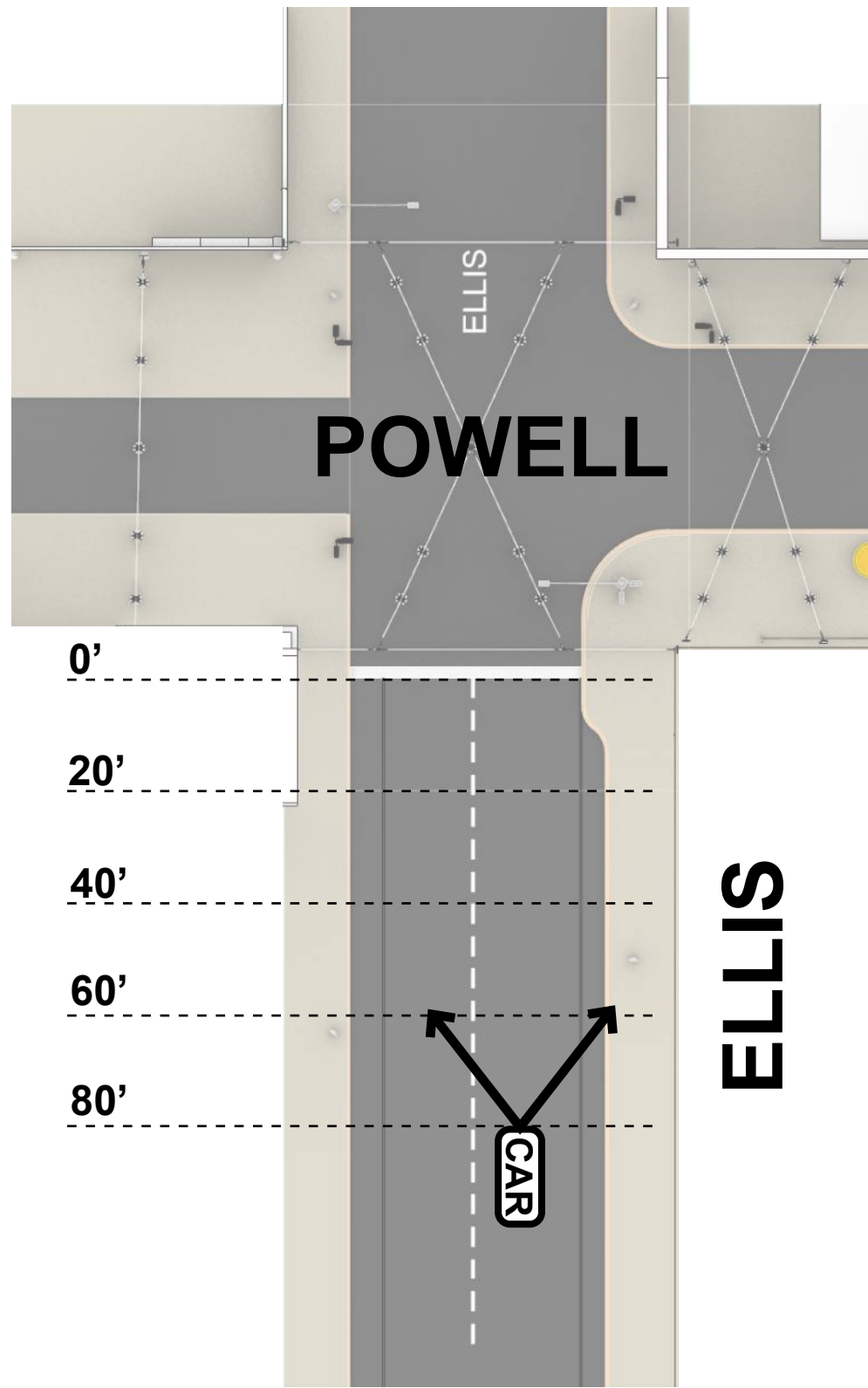
Visual Length



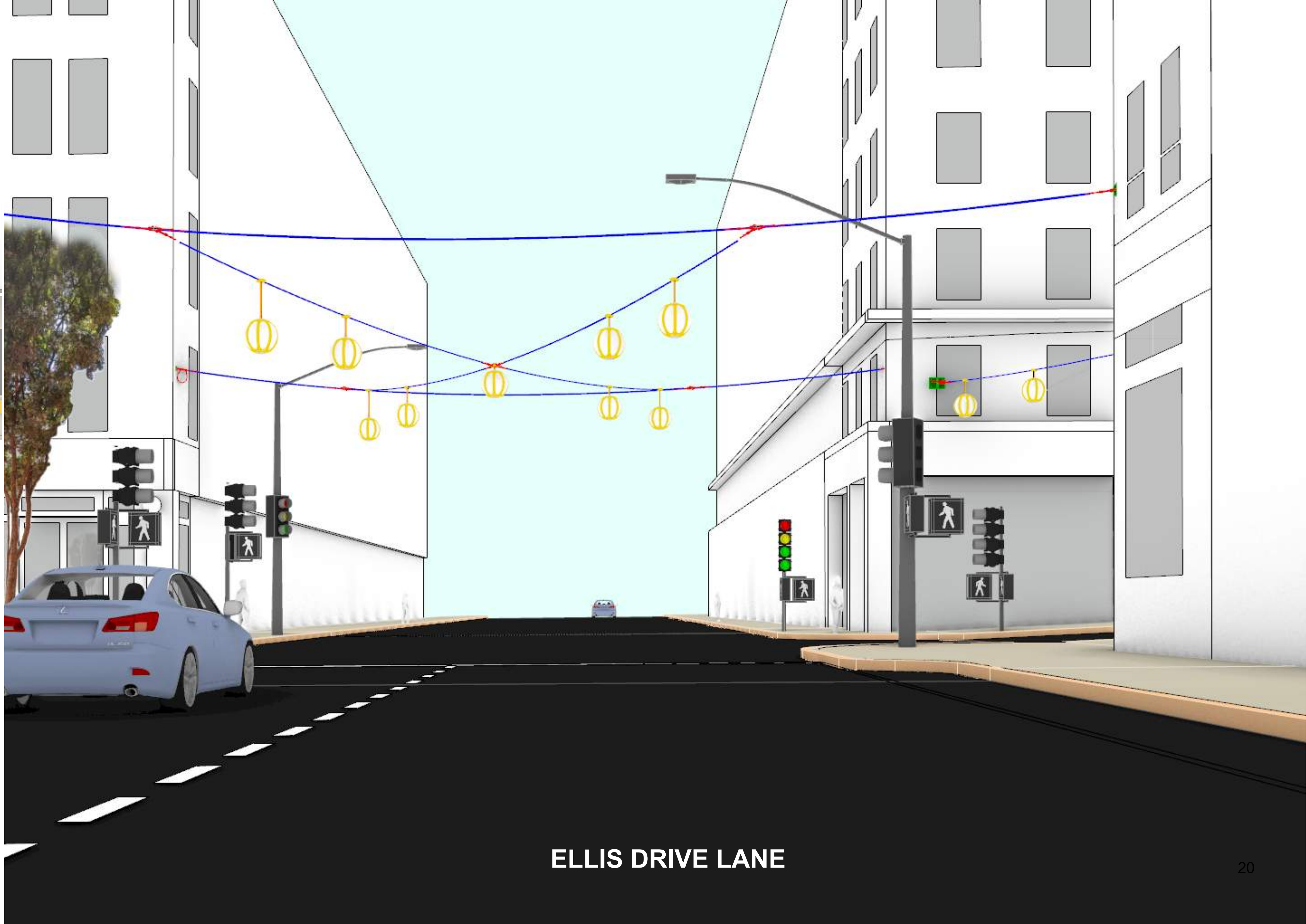
Regular Driver's view.

O'FARRELL DRIVE LANE

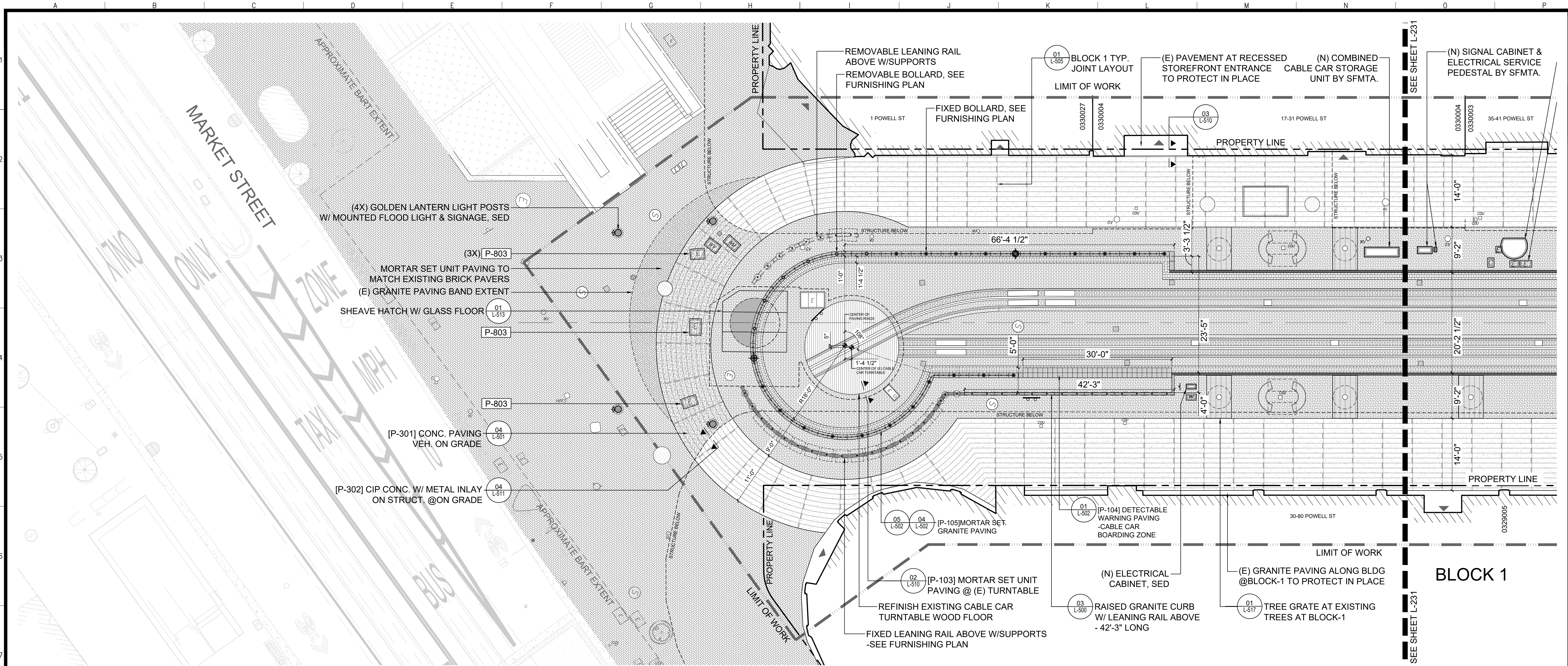
Per the 3D study, all traffic signals are fully visible at Ellis.



Visual Length



ELLIS DRIVE LANE



LEGEND

	CITY STANDARD CONC. CURB & GUTTER, SCD		REFRESHED CITY STANDARD ASPHALT ROADWAY PAVING, IN BLOCK 2 & 3 BTW ELLIS & GEARY ST, SCD		(N) BASEMENT ACCESS HATCH		PLANTER POT W/ FOUNDATION		"GOLDEN LANTERN" POST W/ [C-103] SIGNAGE & POST MOUNTED FLOOD LIGHT, SED		BUILDING ENTRANCE
	[P-201] GRANITE CURB TYP.		[P-301] CITY STANDARD CIP CONC. PAVING VEH. W/ METAL INLAY		CUSTOM DUCTILE IRON TREE GRATE		BENCH W/ FOUNDATION		(E) HISTORIC GOLDEN TRIANGLE LIGHT TO REMAIN, ADJUST ELEVATION TO FINISHED GRADE		(E) UTILITY VALVES TO REMAIN OR RELOCATE, ADJUST TO FINISHED GRADE, SCD
	[P-202] RAISED GRANITE CURB @ BLOCK-1		[P-302] CITY STANDARD CIP CONC. PAVING PED. W/ METAL INLAY		SLIP RESISTANT REMOVABLE CLEAR GLASS FLOOR WITHIN SHEAVE PIT COVER HATCH		[R-101] FIXED LEANING RAIL		RELOCATED (E) HISTORIC GOLDEN TRIANGLE LIGHT		ELECTRICAL CABINET
	[P-203] (E) GRANITE PAVING BAND TO REMAIN		[P-101] BRICK UNIT PAVING VEH. ON (E) SLAB ON GRADE		UTILITY COVER W/ PAVING INFILL, MATERIAL AND SIZE VARIES		[R-102] REMOVABLE LEANING RAIL		(E) STREET LIGHT TO REMAIN		LIMIT OF WORK
	[P-104] DETECTABLE WARNING SURFACE		[P-103] BRICK UNIT PAVING PED.		(E) HISTORIC MARKERS, TO BE REMOVED, STORED, POLISHED & REINSTALLED IN SAME LOCATION.		[SF-101] FIXED BOLLARD W/ CHAIN		(E) STREET LIGHT WITH TRAFFIC SIGNAL TO REMAIN		PROPERTY LINE
	[P-105] GRANITE UNIT PAVING		(E) BRICK PAVING TO REMAIN IN BLOCK-1 ALONG MARKET STREET		SFMTA STANDARD CABLE CAR STOP SIGN		[SF-102] REMOVABLE BOLLARD W/ CHAIN		(E) TRAFFIC SIGNAL TO REMAIN		STRUCTURE BELOW
	REFINISH (E) CABLE CAR TURNTABLE WOOD FLOOR		MORTAR SET BRICK UNIT PAVING TO MATCH (E) BRICK PAVERS ALONG MARKET STREET		TRAFFIC SIGNAGE TO BE COORDINATED WITH SFMTA		[L-101] (E) CABLE CAR TRAFFIC LIGHT TO REMAIN		(E) HIGH OR LOW PRESSURE FIRE HYDRANT TO REMAIN, ADJUST TO FINISHED GRADE, SCD		COMMERCIAL LOADING ZONE
	[SF-104] CITY STANDARD TRASH RECEPTACLE								(E) MANHOLE TO REMAIN OR RELOCATE, ADJUST TO FINISHED GRADE, SCD		

NO.	DATE	DESCRIPTION	BY	APP.
TABLE OF REVISIONS				

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BUREAU OF LANDSCAPE ARCHITECTURE
 CITY & COUNTY OF SAN FRANCISCO
SAN FRANCISCO PUBLIC WORKS
 49 SOUTH VAN NESS AVENUE, SUITE 1000
 SAN FRANCISCO, CA 94103

Section Mgr: TONY ESTERBROOKS
 Bureau Mgr: JENNIFER COOPER

DESIGNED: DATE: _____
 DRAWN: DATE: _____
 CHECKED: DATE: _____

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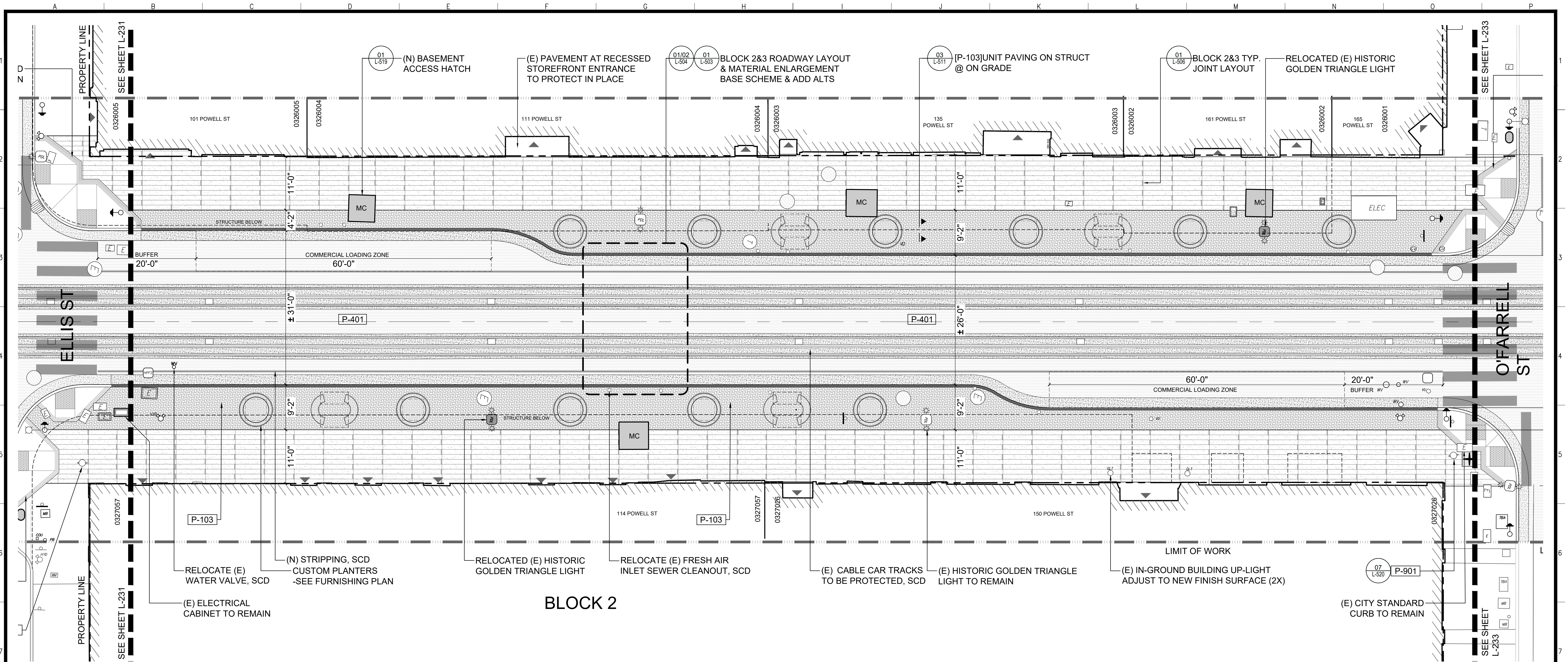
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POWELL STREETSCAPE IMPROVEMENTS PROJECT

MATERIALS PLAN

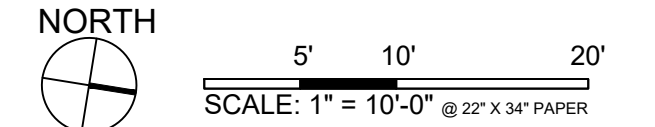


SOURCING EVENT ID. 00000XXXX
 DRAWING NO. L-230
 FILE NO.
 REV. NO.



LEGEND

- | | | | | | |
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| <p>01/02 L-500 [P-201] GRANITE CURB TYP.</p> <p>03 L-500 [P-202] RAISED GRANITE CURB @ BLOCK-1</p> <p>[P-203] (E) GRANITE PAVING BAND TO REMAIN</p> <p>01 L-502 02 L-502 [P-104] DETECTABLE WARNING SURFACE</p> <p>04 L-502 05 L-502 [P-105] GRANITE UNIT PAVING</p> <p>01 L-706 [SF-104] CITY STANDARD TRASH RECEPTACLE</p> | <p>01 L-503 REFRESHED CITY STANDARD ASPHALT ROADWAY PAVING, IN BLOCK 2 & 3 BTW ELLIS & GEARY ST, SCD
ADD ALT-1&2: VEH. UNIT PAVERS MORTAR SET ON CONC. SUB-SLABS.</p> <p>01 L-519 [P-301] CITY STANDARD CIP CONC. PAVING VEH. W/ METAL INLAY</p> <p>01 L-517 [P-801] CITY STANDARD CIP CONC. PAVING PED. W/ METAL INLAY</p> <p>01 L-513 [P-802] CITY STANDARD CIP CONC. PAVING VEH. W/ METAL INLAY</p> <p>01 L-515 [P-803] BRICK UNIT PAVING VEH. ON (E) SLAB ON GRADE</p> <p>01 L-520 [P-901] BRICK UNIT PAVING PED.</p> <p>06 L-501 05 L-501 08 L-501 [P-103] MORTAR SET BRICK UNIT PAVING TO MATCH (E) BRICK PAVERS ALONG MARKET STREET</p> | <p>MC (N) BASEMENT ACCESS HATCH</p> <p>CUSTOM DUCTILE IRON TREE GRATE</p> <p>SLIP RESISTANT REMOVABLE CLEAR GLASS FLOOR WITHIN SHEAVE PIT COVER HATCH</p> <p>UTILITY COVER W/ PAVING INFILL, MATERIAL AND SIZE VARIES</p> <p>(E) HISTORIC MARKERS, TO BE REMOVED, STORED, POLISHED & REINSTALLED IN SAME LOCATION.</p> <p>SFMTA STANDARD CABLE CAR STOP SIGN</p> <p>TRAFFIC SIGNAGE TO BE COORDINATED WITH SFMTA</p> | <p>02 L-702 01 L-702 [C-101] PLANTER POT W/ FOUNDATION</p> <p>03 L-701 02 L-700 [C-102] BENCH W/ FOUNDATION</p> <p>01/02 L-704 04 L-704 [R-101] FIXED LEANING RAIL</p> <p>03 L-704 06 L-704 [R-102] REMOVABLE LEANING RAIL</p> <p>01 L-705 02 L-705 [SF-101] FIXED BOLLARD W/ CHAIN</p> <p>02 L-705 04/05 L-705 [SF-102] REMOVABLE BOLLARD W/ CHAIN</p> <p>[L-101] (E) CABLE CAR TRAFFIC LIGHT TO REMAIN</p> | <p>01 L-800 "GOLDEN LANTERN" POST W/ [C-103] SIGNAGE & POST MOUNTED FLOOD LIGHT, SED</p> <p>01 L-750 (E) HISTORIC GOLDEN TRIANGLE LIGHT TO REMAIN, ADJUST ELEVATION TO FINISHED GRADE</p> <p>01 L-750 RELOCATED (E) HISTORIC GOLDEN TRIANGLE LIGHT</p> <p>(E) STREET LIGHT TO REMAIN</p> <p>(E) STREET LIGHT WITH TRAFFIC SIGNAL TO REMAIN</p> <p>(E) TRAFFIC SIGNAL TO REMAIN</p> <p>(E) HIGH OR LOW PRESSURE FIRE HYDRANT TO REMAIN, ADJUST TO FINISHED GRADE, SCD</p> <p>01 L-516 (E) MANHOLE TO REMAIN OR RELOCATE, ADJUST TO FINISHED GRADE, SCD</p> | <p>BUILDING ENTRANCE</p> <p>(E) DRAIN TO REMAIN OR RELOCATE, ADJUST TO FINISHED GRADE, SCD</p> <p>(E) UTILITY VALVES TO REMAIN OR RELOCATE, ADJUST TO FINISHED GRADE, SCD</p> <p>ELECTRICAL CABINET</p> <p>LIMIT OF WORK</p> <p>PROPERTY LINE</p> <p>STRUCTURE BELOW</p> <p>COMMERCIAL LOADING ZONE</p> |
|--|--|--|--|---|---|



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CITY & COUNTY OF SAN FRANCISCO
SAN FRANCISCO PUBLIC WORKS
49 SOUTH VAN NESS AVENUE, SUITE 1000
SAN FRANCISCO, CA 94103

Section Mgr:	TONY ESTERBROOKS	Date:	
Bureau Mgr:	JENNIFER COOPER	DESIGNED:	DATE:
		DRAWN:	DATE:
		CHECKED:	DATE:

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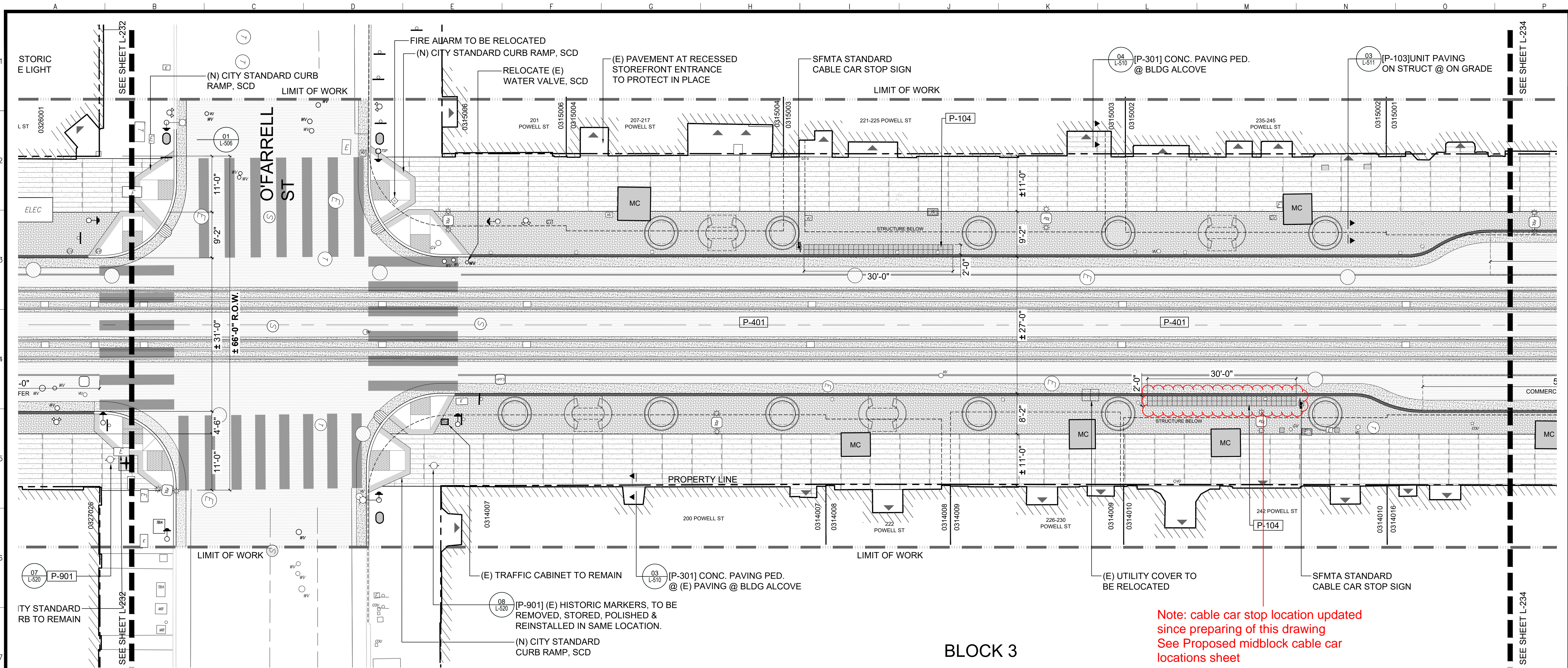
SCALE:
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47/170

POWELL STREETSCAPE IMPROVEMENTS PROJECT

MATERIALS PLAN



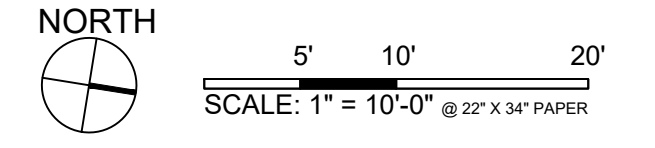
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DRAWING NO. **L-232**
FILE NO.
REV. NO.



Note: cable car stop location updated since preparing of this drawing
See Proposed midblock cable car locations sheet

BLOCK 3

LEGEND		LEGEND		LEGEND		LEGEND	
	CITY STANDARD CONC. CURB & GUTTER, SCD		REFRESHED CITY STANDARD ASPHALT ROADWAY PAVING, IN BLOCK 2 & 3 BTW ELLIS & GEARY ST, SCD		(N) BASEMENT ACCESS HATCH		"GOLDEN LANTERN" POST W/ [C-103] SIGNAGE & POST MOUNTED FLOOD LIGHT, SED
	GRANITE CURB TYP.		CUSTOM DUCTILE IRON TREE GRATE		SLIP RESISTANT REMOVABLE CLEAR GLASS FLOOR WITHIN SHEAVE PIT COVER HATCH		(E) HISTORIC GOLDEN TRIANGLE LIGHT TO REMAIN, ADJUST ELEVATION TO FINISHED GRADE
	RAISED GRANITE CURB @ BLOCK-1		CITY STANDARD CIP CONC. PAVING VEH. W/ METAL INLAY		UTILITY COVER W/ PAVING INFILL, MATERIAL AND SIZE VARIES		RELOCATED (E) HISTORIC GOLDEN TRIANGLE LIGHT
	(E) GRANITE PAVING BAND TO REMAIN		CITY STANDARD CIP CONC. PAVING PED. W/ METAL INLAY		(E) HISTORIC MARKERS, TO BE REMOVED, STORED, POLISHED & REINSTALLED IN SAME LOCATION.		(E) STREET LIGHT TO REMAIN
	DETECTABLE WARNING SURFACE		BRICK UNIT PAVING VEH. ON (E) SLAB ON GRADE		FIXED LEANING RAIL		(E) STREET LIGHT WITH TRAFFIC SIGNAL TO REMAIN
	GRANITE UNIT PAVING		BRICK UNIT PAVING PED.		REMOVABLE LEANING RAIL		(E) TRAFFIC SIGNAL TO REMAIN
	REFINISH (E) CABLE CAR TURNTABLE WOOD FLOOR		MORTAR SET BRICK UNIT PAVING TO MATCH (E) BRICK PAVERS ALONG MARKET STREET		FIXED BOLLARD W/ CHAIN		(E) HIGH OR LOW PRESSURE FIRE HYDRANT TO REMAIN, ADJUST TO FINISHED GRADE, SCD
	CITY STANDARD TRASH RECEPTACLE		MORTAR SET BRICK UNIT PAVING TO MATCH (E) BRICK PAVERS ALONG MARKET STREET		REMOVABLE BOLLARD W/ CHAIN		(E) MANHOLE TO REMAIN OR RELOCATE, ADJUST TO FINISHED GRADE, SCD
	(N) CITY STANDARD CURB RAMP, SCD		(E) CABLE CAR TRAFFIC LIGHT TO REMAIN		BUILDING ENTRANCE		(E) DRAIN TO REMAIN OR RELOCATE, ADJUST TO FINISHED GRADE, SCD
	(E) GRANITE PAVING BAND TO REMAIN		SFMTA STANDARD CABLE CAR STOP SIGN		(E) UTILITY VALVES TO REMAIN OR RELOCATE, ADJUST TO FINISHED GRADE, SCD		ELECTRICAL CABINET
	GRANITE UNIT PAVING		TRAFFIC SIGNAGE TO BE COORDINATED WITH SFMTA		LIMIT OF WORK		PROPERTY LINE
	REFINISH (E) CABLE CAR TURNTABLE WOOD FLOOR		STRUCTURE BELOW		COMMERCIAL LOADING ZONE		



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49 SOUTH VAN NESS AVENUE, SUITE 1000
SAN FRANCISCO, CA 94103

Section Mgr:	TONY ESTERBROOKS	Date:	
Bureau Mgr:	JENNIFER COOPER	DESIGNED:	DATE:
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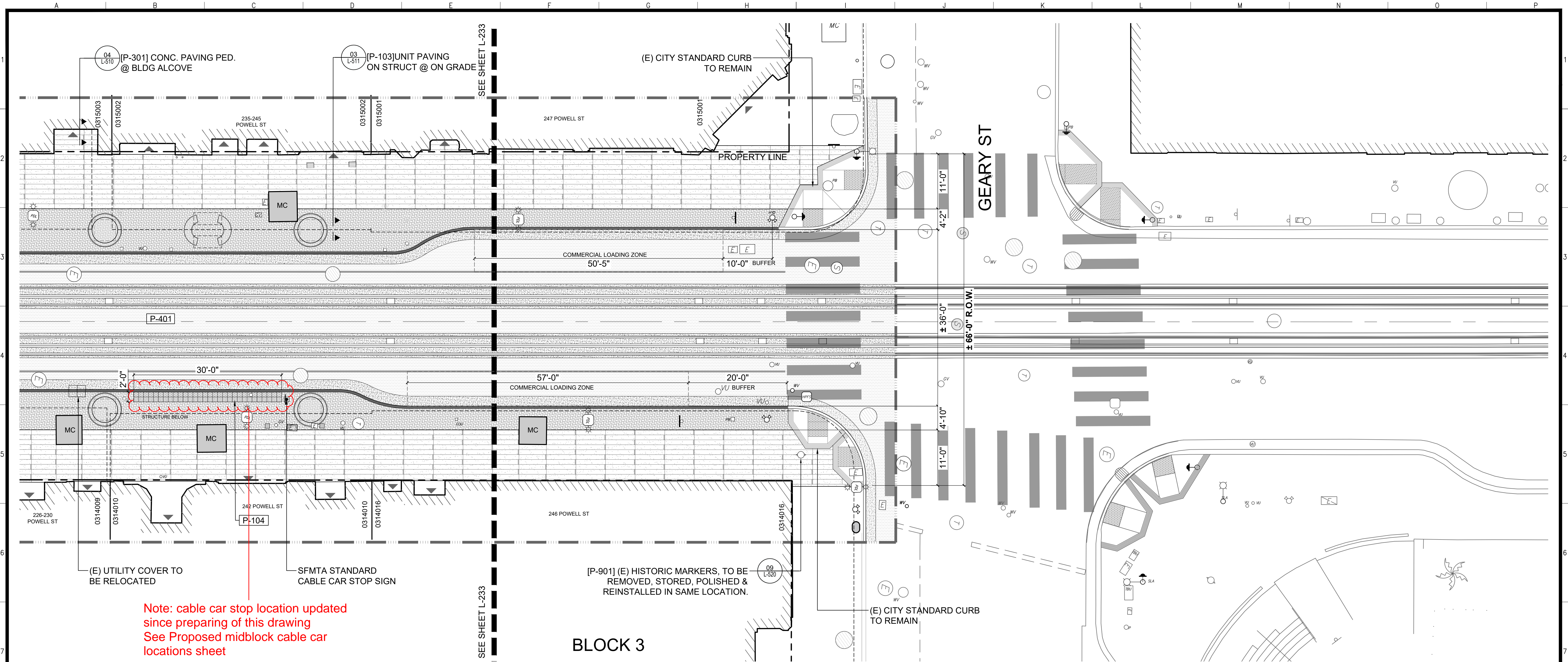
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POWELL STREETSCAPE IMPROVEMENTS PROJECT

MATERIALS PLAN



SOURCING EVENT ID. 000000XXXX
DRAWING NO. **L-233**
FILE NO.
REV. NO.



LEGEND

	CITY STANDARD CONC. CURB & GUTTER, SCD		REFRESHED CITY STANDARD ASPHALT ROADWAY PAVING, IN BLOCK 2 & 3 BTW ELLIS & GEARY ST, SCD ADD ALT-182: VEH. UNIT PAVERS MORTAR SET ON CONC. SUB-SLABS.		(N) BASEMENT ACCESS HATCH		PLANTER POT W/ FOUNDATION		"GOLDEN LANTERN" POST W/ [C-103] SIGNAGE & POST MOUNTED FLOOD LIGHT, SED		BUILDING ENTRANCE
	GRANITE CURB TYP.		CITY STANDARD CIP CONC. PAVING VEH. W/ METAL INLAY		CUSTOM DUCTILE IRON TREE GRATE		BENCH W/ FOUNDATION		(E) HISTORIC GOLDEN TRIANGLE LIGHT TO REMAIN, ADJUST ELEVATION TO FINISHED GRADE		RELOCATED (E) HISTORIC GOLDEN TRIANGLE LIGHT
	RAISED GRANITE CURB @ BLOCK-1		CITY STANDARD CIP CONC. PAVING PED. W/ METAL INLAY		SLIP RESISTANT REMOVABLE CLEAR GLASS FLOOR WITHIN SHEAVE PIT COVER HATCH		FIXED LEANING RAIL		(E) STREET LIGHT TO REMAIN		(E) STREET LIGHT WITH TRAFFIC SIGNAL TO REMAIN
	(E) GRANITE PAVING BAND TO REMAIN		BRICK UNIT PAVING VEH. ON (E) SLAB ON GRADE		UTILITY COVER W/ PAVING INFILL, MATERIAL AND SIZE VARIES		REMOVABLE LEANING RAIL		(E) TRAFFIC SIGNAL TO REMAIN		LIMIT OF WORK
	DETECTABLE WARNING SURFACE		MORTAR SET BRICK UNIT PAVING ON (E) SLAB @ BLOCK-1		(E) HISTORIC MARKERS, TO BE REMOVED, STORED, POLISHED & REINSTALLED IN SAME LOCATION.		FIXED BOLLARD W/ CHAIN		ELECTRICAL CABINET		PROPERTY LINE
	GRANITE UNIT PAVING		BRICK UNIT PAVING PED.		SFMTA STANDARD CABLE CAR STOP SIGN		REMOVABLE BOLLARD W/ CHAIN		STRUCTURE BELOW		COMMERCIAL LOADING ZONE
	REFINISH (E) CABLE CAR TURNTABLE WOOD FLOOR		(E) BRICK PAVING TO REMAIN IN BLOCK-1 ALONG MARKET STREET		MORTAR SET BRICK UNIT PAVING TO MATCH (E) BRICK PAVERS ALONG MARKET STREET		SFMTA STANDARD CABLE CAR STOP SIGN		(E) MANHOLE TO REMAIN OR RELOCATE, ADJUST TO FINISHED GRADE, SCD		
	CITY STANDARD TRASH RECEPTACLE				TRAFFIC SIGNAGE TO BE COORDINATED WITH SFMTA		(E) CABLE CAR TRAFFIC LIGHT TO REMAIN		(E) HIGH OR LOW PRESSURE FIRE HYDRANT TO REMAIN, ADJUST TO FINISHED GRADE, SCD		

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 CITY & COUNTY OF SAN FRANCISCO
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 SAN FRANCISCO, CA 94103

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Bureau Mgr:	JENNIFER COOPER	DESIGNED:	DATE:
		DRAWN:	DATE:
		CHECKED:	DATE:

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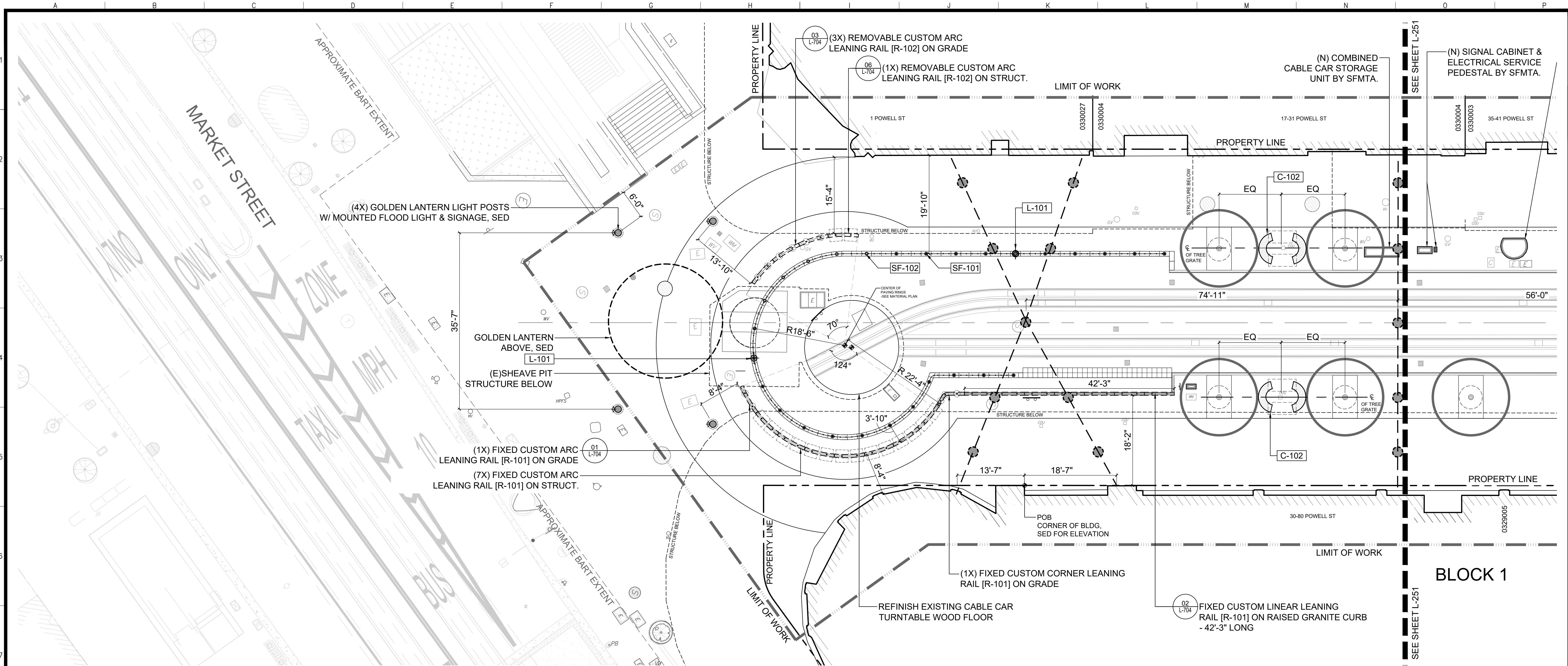
SCALE:
SHEET OF SHEETS
49/170

POWELL STREETSCAPE IMPROVEMENTS PROJECT

MATERIALS PLAN

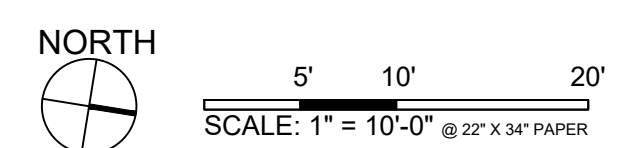


SOURCING EVENT ID. 00000XXXX
 DRAWING NO. **L-234**
 FILE NO.
 REV. NO.



LEGEND

- | | | | | |
|---|--|---|---|---|
| <p>02 L-702 01 L-702 [C-101] PLANTER POT W/ FOUNDATION</p> <p>03 L-704 02 L-700 [C-102] BENCH W/ FOUNDATION</p> <p>01/02 L-704 04 L-704 [R-101] FIXED LEANING RAIL</p> <p>03 L-704 06 L-704 [R-102] REMOVABLE LEANING RAIL</p> <p>01 L-705 02 L-705 [SF-101] FIXED BOLLARD W/ CHAIN</p> <p>02 L-705 04/05 L-705 [SF-102] REMOVABLE BOLLARD W/ CHAIN</p> | <p>01 L-800 "GOLDEN LANTERN" POST W/ [C-103] SIGNAGE & POST MOUNTED FLOOD LIGHT, SED</p> <p>01 L-520 [P-901] CATENARY LIGHTING CABLES & SUSPENDED PENDANTS ABOVE, SED</p> <p>01 L-520 [P-901] (E) HISTORIC MARKERS, TO BE REMOVED, STORED, POLISHED & REINSTALLED IN SAME LOCATION.</p> <p>[SF-102] SFMTA STANDARD CABLE CAR STOP SIGN</p> <p>[SF-103] TRAFFIC SIGNAGE TO BE COORDINATED WITH SFMTA</p> <p>01 L-706 [SF-104] CITY STANDARD TRASH RECEPTACLE</p> <p>[L-101] (E) CABLE CAR TRAFFIC LIGHT TO REMAIN</p> | <p>01 L-750 (E) HISTORIC GOLDEN TRIANGLE LIGHT TO REMAIN, ADJUST ELEVATION TO FINISHED GRADE</p> <p>01 L-750 RELOCATED (E) HISTORIC GOLDEN TRIANGLE LIGHT</p> <p>(E) STREET LIGHT TO REMAIN</p> <p>(E) STREET LIGHT WITH TRAFFIC SIGNAL TO REMAIN</p> <p>(E) TRAFFIC SIGNAL TO REMAIN</p> <p>03 L-900 STREET TREES IN PLANTER POT AT BLOCK 2&3</p> <p>01 L-519 (N) BASEMENT ACCESS HATCH</p> <p>01 L-516 (E) MANHOLE TO REMAIN OR RELOCATE, ADJUST TO FINISHED GRADE, SCD</p> | <p>(E) IN-GROUND EXISTING STREET TREES AT BLOCK-1</p> <p>(E) HIGH OR LOW PRESSURE FIRE HYDRANT TO REMAIN, ADJUST TO FINISHED GRADE, SCD</p> <p>BUILDING ENTRANCE</p> <p>(E) UTILITY VALVES TO REMAIN OR RELOCATE, ADJUST TO FINISHED GRADE, SCD</p> <p>(E) DRAIN TO REMAIN OR RELOCATE, ADJUST TO FINISHED GRADE, SCD</p> <p>ELECTRICAL CABINET</p> | <p>LIMIT OF WORK</p> <p>PROPERTY LINE</p> <p>STRUCTURE BELOW</p> <p>COMMERCIAL LOADING ZONE</p> |
|---|--|---|---|---|



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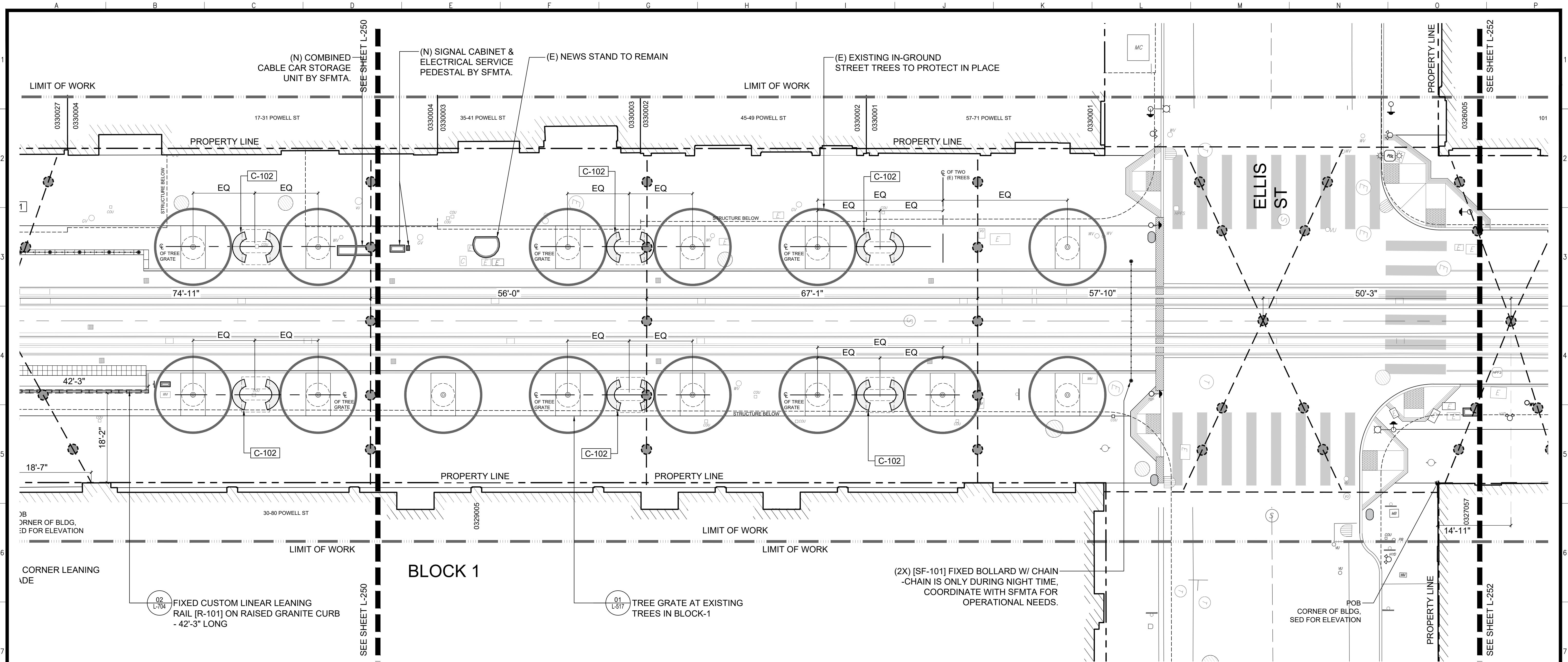
SCALE:
SHEET OF SHEETS
50/170

POWELL STREETSCAPE IMPROVEMENTS PROJECT

FURNISHING PLAN



SOURCING EVENT ID. 00000XXXX
 DRAWING NO. **L-250**
 FILE NO.
 REV. NO.



LEGEND

[C-101] PLANTER POT W/ FOUNDATION	"GOLDEN LANTERN" POST W/ [C-103] SIGNAGE & POST MOUNTED FLOOD LIGHT, SED	(E) HISTORIC GOLDEN TRIANGLE LIGHT TO REMAIN, ADJUST ELEVATION TO FINISHED GRADE	(E) IN-GROUND EXISTING STREET TREES AT BLOCK-1	(E) HIGH OR LOW PRESSURE FIRE HYDRANT TO REMAIN, ADJUST TO FINISHED GRADE, SCD	LIMIT OF WORK
[C-102] BENCH W/ FOUNDATION	CATENARY LIGHTING CABLES & SUSPENDED PENDANTS ABOVE, SED	RELOCATED (E) HISTORIC GOLDEN TRIANGLE LIGHT	(E) STREET LIGHT TO REMAIN	BUILDING ENTRANCE	STRUCTURE BELOW
[R-101] FIXED LEANING RAIL	(E) HISTORIC MARKERS, TO BE REMOVED, STORED, POLISHED & REINSTALLED IN SAME LOCATION.	(E) STREET LIGHT WITH TRAFFIC SIGNAL TO REMAIN	STREET TREES IN PLANTER POT AT BLOCK 2&3	(E) UTILITY VALVES TO REMAIN OR RELOCATE, ADJUST TO FINISHED GRADE, SCD	COMMERCIAL LOADING ZONE
[R-102] REMOVABLE LEANING RAIL	SFMTA STANDARD CABLE CAR STOP SIGN	(E) TRAFFIC SIGNAL TO REMAIN	(E) MANHOLE TO REMAIN OR RELOCATE, ADJUST TO FINISHED GRADE, SCD	(E) DRAIN TO REMAIN OR RELOCATE, ADJUST TO FINISHED GRADE, SCD	ELECTRICAL CABINET
[SF-101] FIXED BOLLARD W/ CHAIN	TRAFFIC SIGNAGE TO BE COORDINATED WITH SFMTA	CITY STANDARD TRASH RECEPTACLE	(N) BASEMENT ACCESS HATCH		
[SF-102] REMOVABLE BOLLARD W/ CHAIN	(E) CABLE CAR TRAFFIC LIGHT TO REMAIN				

NORTH
SCALE: 1" = 10'-0" @ 22" x 34" PAPER

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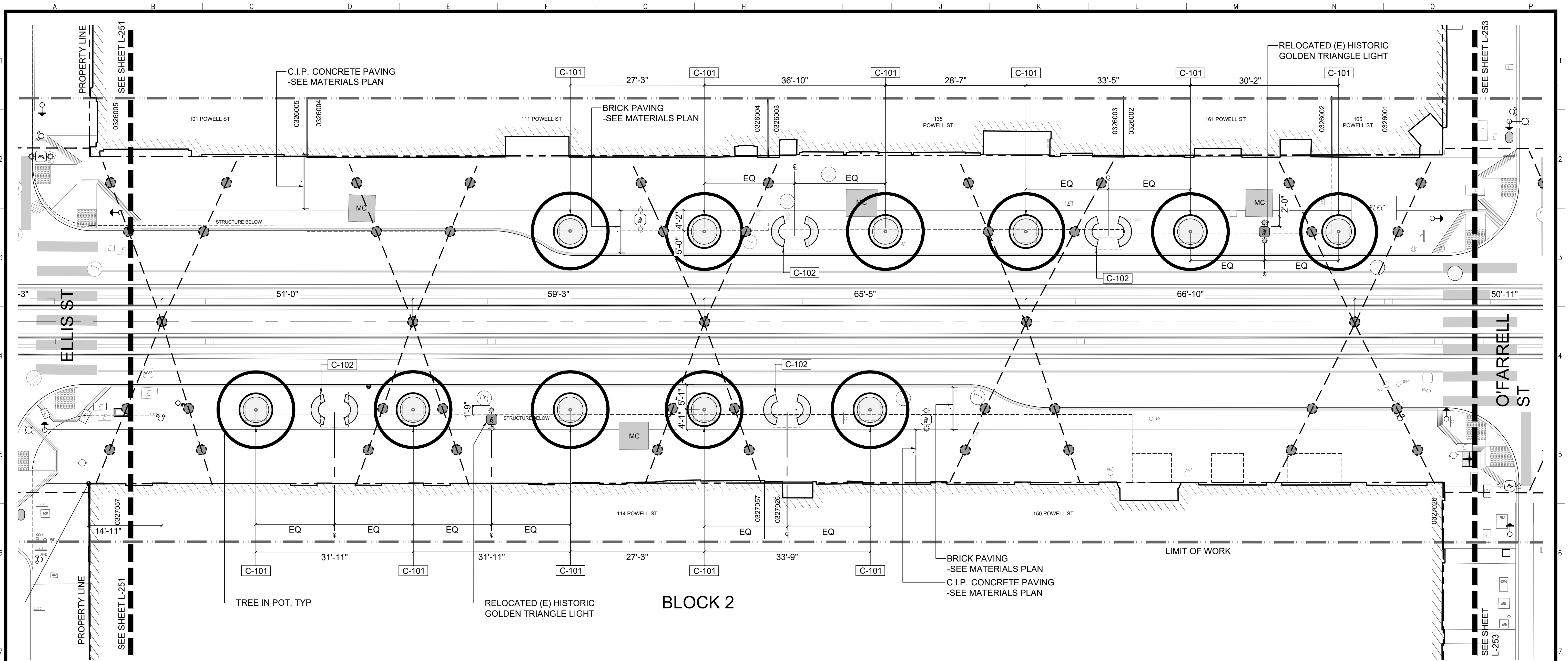
NOT FOR CONSTRUCTION

SCALE:
SHEET OF SHEETS
51/170

POWELL STREETSCAPE IMPROVEMENTS PROJECT
FURNISHING PLAN



SOURCING EVENT ID. 00000XXXX
DRAWING NO. **L-251**
FILE NO.
REV. NO.



BLOCK 2

LEGEND

[C-101] PLANTER POT W/ FOUNDATION	"GOLDEN LANTERN" POST W/ [C-103] SIGNAGE & POST MOUNTED FLOOD LIGHT, SED	(E) IN-GROUND EXISTING STREET TREES AT BLOCK-1	(E) HIGH OR LOW PRESSURE FIRE HYDRANT TO REMAIN, ADJUST TO FINISHED GRADE, SCD
[C-102] BENCH W/ FOUNDATION	CATENARY LIGHTING CABLES & SUSPENDED PENDANTS ABOVE, SED	RELOCATED (E) HISTORIC GOLDEN TRIANGLE LIGHT	BUILDING ENTRANCE
[R-101] FIXED LEANING RAIL	(E) HISTORIC MARKERS, TO BE REMOVED, STORED, POLISHED & REINSTALLED IN SAME LOCATION.	(E) STREET LIGHT TO REMAIN	(E) UTILITY VALVES TO REMAIN OR RELOCATE, ADJUST TO FINISHED GRADE, SCD
[R-102] REMOVABLE LEANING RAIL	SFMTA STANDARD CABLE CAR STOP SIGN	(E) STREET LIGHT WITH TRAFFIC SIGNAL TO REMAIN	(E) DRAIN TO REMAIN OR RELOCATE, ADJUST TO FINISHED GRADE, SCD
[SF-101] FIXED BOLLARD W/ CHAIN	TRAFFIC SIGNAGE TO BE COORDINATED WITH SFMTA	(E) TRAFFIC SIGNAL TO REMAIN	ELECTRICAL CABINET
[SF-102] REMOVABLE BOLLARD W/ CHAIN	CITY STANDARD TRASH RECEPTACLE	(E) MANHOLE TO REMAIN OR RELOCATE, ADJUST TO FINISHED GRADE, SCD	
	(E) CABLE CAR TRAFFIC LIGHT TO REMAIN		

LIMIT OF WORK
 PROPERTY LINE
 STRUCTURE BELOW
 COMMERCIAL LOADING ZONE

NORTH
 SCALE: 1" = 10'-0" @ 22" x 34" PAPER

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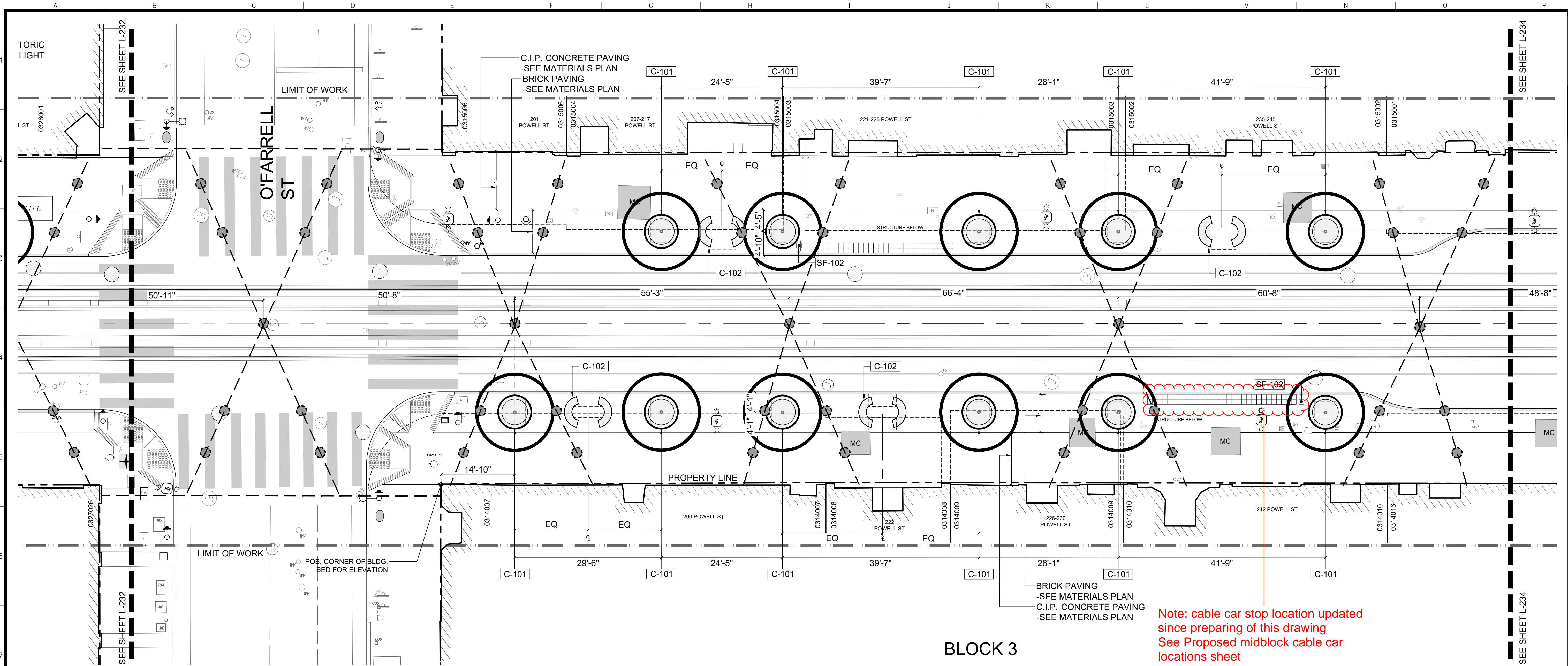
SCALE:
SHEET OF SHEETS
52/170

POWELL STREETSCAPE IMPROVEMENTS PROJECT

FURNISHING PLAN



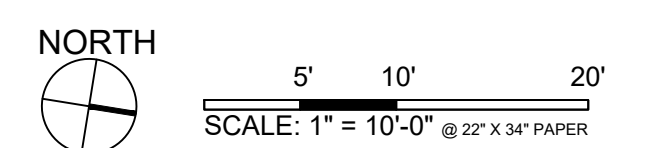
SOURCING EVENT ID. 00000XXXX
 DRAWING NO. **L-252**
 FILE NO.
 REV. NO.



BLOCK 3

LEGEND

- | | | | | |
|--|--|--|---|--|
| <ul style="list-style-type: none"> [C-101] PLANTER POT W/ FOUNDATION [C-102] BENCH W/ FOUNDATION [R-101] FIXED LEANING RAIL [R-102] REMOVABLE LEANING RAIL [SF-101] FIXED BOLLARD W/ CHAIN [SF-102] REMOVABLE BOLLARD W/ CHAIN | <ul style="list-style-type: none"> [L-702] "GOLDEN LANTERN" POST W/ MOUNTED FLOOD LIGHT, SED [L-700] CATENARY LIGHTING CABLES & SUSPENDED PENDANTS ABOVE, SED [P-901] (E) HISTORIC MARKERS, TO BE REMOVED, STORED, POLISHED & REINSTALLED IN SAME LOCATION. [SF-102] SFMTA STANDARD CABLE CAR STOP SIGN [SF-103] TRAFFIC SIGNAGE TO BE COORDINATED WITH SFMTA [L-705] CITY STANDARD TRASH RECEPTACLE [L-705] (E) CABLE CAR TRAFFIC LIGHT TO REMAIN | <ul style="list-style-type: none"> [L-750] (E) HISTORIC GOLDEN TRIANGLE LIGHT TO REMAIN, ADJUST ELEVATION TO FINISHED GRADE [L-750] RELOCATED (E) HISTORIC GOLDEN TRIANGLE LIGHT [L-750] (E) STREET LIGHT TO REMAIN [L-750] (E) STREET LIGHT WITH TRAFFIC SIGNAL TO REMAIN [L-750] (E) TRAFFIC SIGNAL TO REMAIN [L-519] (E) MANHOLE TO REMAIN OR RELOCATE, ADJUST TO FINISHED GRADE, SCD | <ul style="list-style-type: none"> [L-900] (E) IN-GROUND EXISTING STREET TREES AT BLOCK-1 [L-900] STREET TREES IN PLANTER POT AT BLOCK 2&3 [L-900] (E) HIGH OR LOW PRESSURE FIRE HYDRANT TO REMAIN, ADJUST TO FINISHED GRADE, SCD [L-900] BUILDING ENTRANCE [L-900] (E) UTILITY VALVES TO REMAIN OR RELOCATE, ADJUST TO FINISHED GRADE, SCD [L-900] (E) DRAIN TO REMAIN OR RELOCATE, ADJUST TO FINISHED GRADE, SCD [L-900] ELECTRICAL CABINET | <ul style="list-style-type: none"> LIMIT OF WORK PROPERTY LINE STRUCTURE BELOW COMMERCIAL LOADING ZONE |
|--|--|--|---|--|



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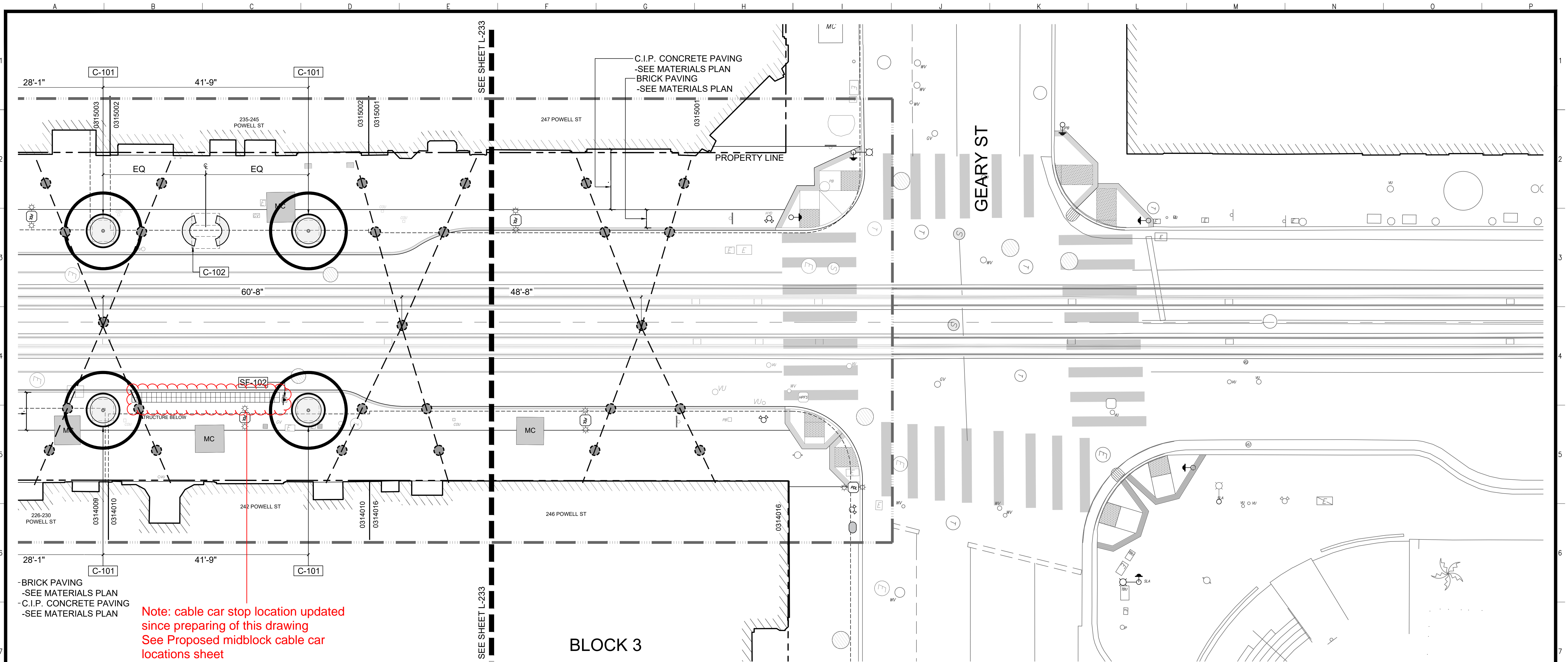
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SCALE:
SHEET OF SHEETS
53/170

POWELL STREETSCAPE IMPROVEMENTS PROJECT
FURNISHING PLAN

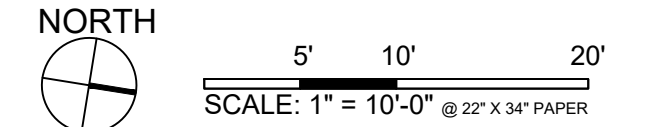


SOURCING EVENT ID. 000000XXXX
DRAWING NO. **L-253**
FILE NO.
REV. NO.



LEGEND

	[C-101]	PLANTER POT W/ FOUNDATION		"GOLDEN LANTERN" POST W/ [C-103] SIGNAGE & POST MOUNTED FLOOD LIGHT, SED		(E) IN-GROUND EXISTING STREET TREES AT BLOCK-1		(E) HIGH OR LOW PRESSURE FIRE HYDRANT TO REMAIN, ADJUST TO FINISHED GRADE, SCD		LIMIT OF WORK
	[C-102]	BENCH W/ FOUNDATION		CATENARY LIGHTING CABLES & SUSPENDED PENDANTS ABOVE, SED		RELOCATED (E) HISTORIC GOLDEN TRIANGLE LIGHT		BUILDING ENTRANCE		PROPERTY LINE
	[R-101]	FIXED LEANING RAIL		(E) HISTORIC MARKERS, TO BE REMOVED, STORED, POLISHED & REINSTALLED IN SAME LOCATION.		(E) STREET LIGHT TO REMAIN		(E) UTILITY VALVES TO REMAIN OR RELOCATE, ADJUST TO FINISHED GRADE, SCD		STRUCTURE BELOW
	[R-102]	REMOVABLE LEANING RAIL		SFMTA STANDARD CABLE CAR STOP SIGN		(E) STREET LIGHT WITH TRAFFIC SIGNAL TO REMAIN		(E) DRAIN TO REMAIN OR RELOCATE, ADJUST TO FINISHED GRADE, SCD		COMMERCIAL LOADING ZONE
	[SF-101]	FIXED BOLLARD W/ CHAIN		TRAFFIC SIGNAGE TO BE COORDINATED WITH SFMTA		(E) TRAFFIC SIGNAL TO REMAIN		ELECTRICAL CABINET		
	[SF-102]	REMOVABLE BOLLARD W/ CHAIN		CITY STANDARD TRASH RECEPTACLE		(E) MANHOLE TO REMAIN OR RELOCATE, ADJUST TO FINISHED GRADE, SCD				
				(E) CABLE CAR TRAFFIC LIGHT TO REMAIN		(N) BASEMENT ACCESS HATCH				



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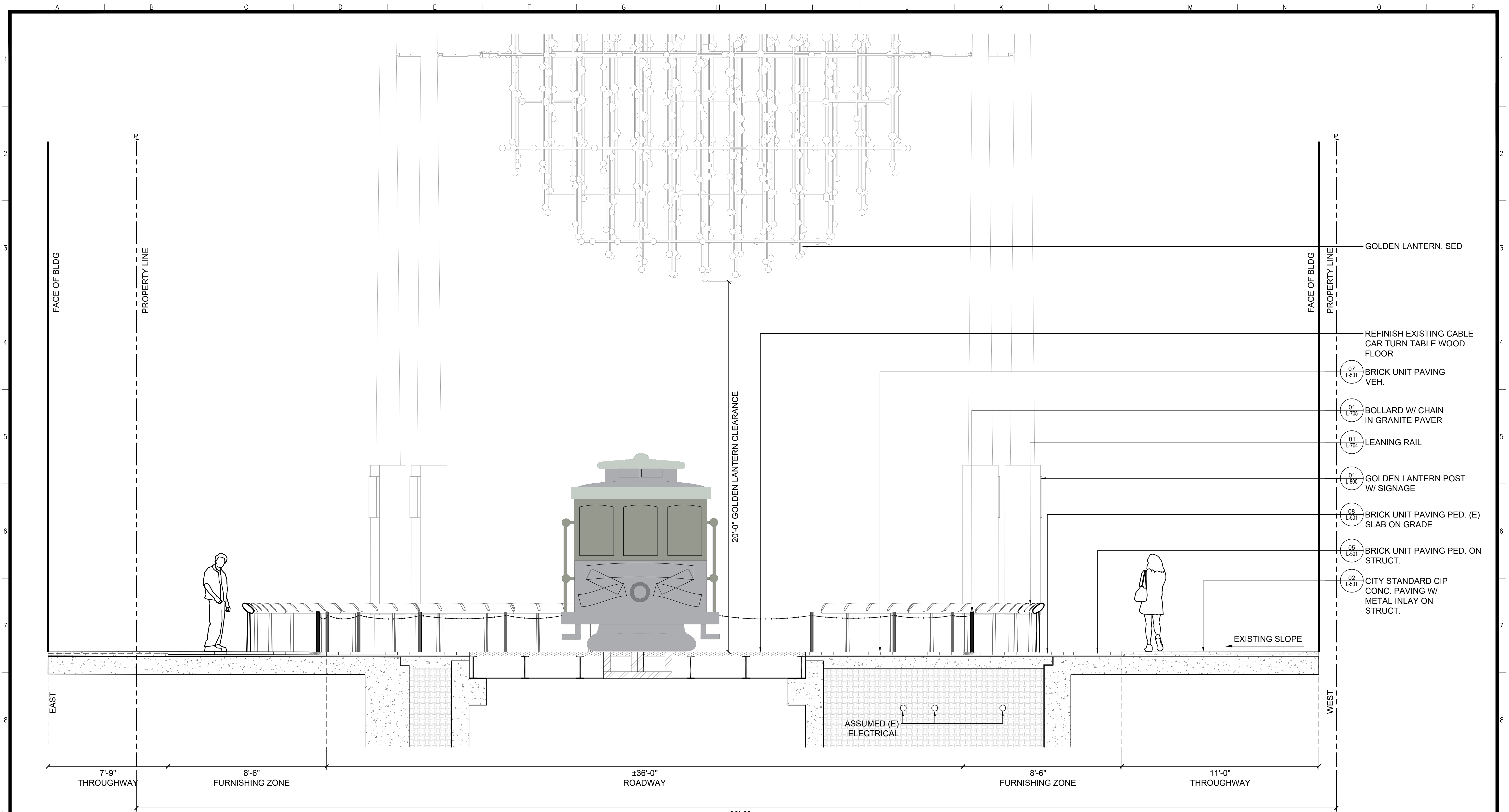
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SCALE:
SHEET OF SHEETS
54/170

POWELL STREETSCAPE IMPROVEMENTS PROJECT
FURNISHING PLAN



SOURCING EVENT ID. 000000XXXX
DRAWING NO. **L-254**
FILE NO.
REV. NO.



01 BLOCK 1 - TURNAROUND
SECTION

3/8" = 1'-0"

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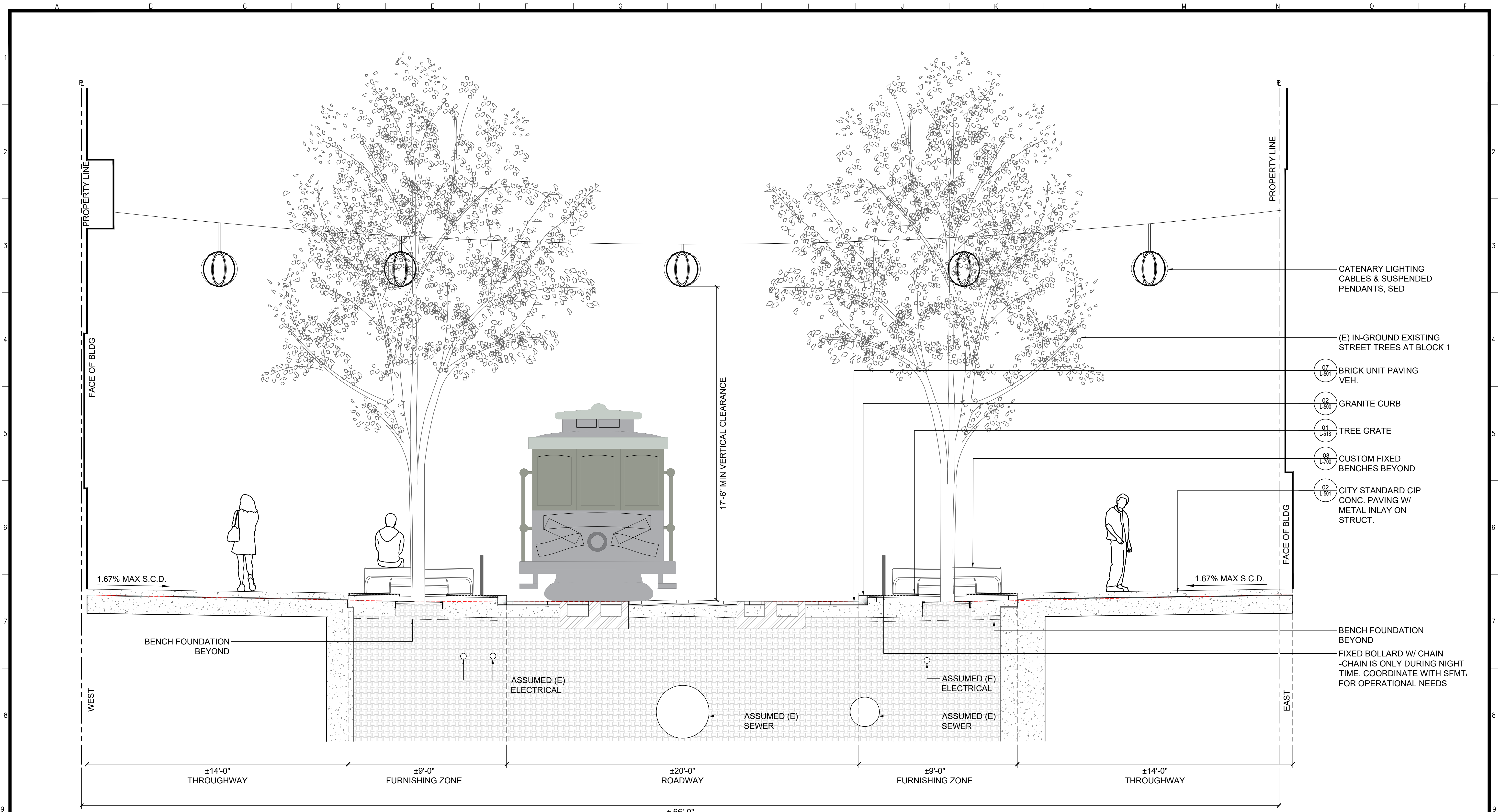
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SCALE:
SHEET OF SHEETS
55/170

POWELL STREETSCAPE IMPROVEMENTS PROJECT
SECTIONS AND ELEVATIONS



SOURCING EVENT ID.
000000XXXX
DRAWING NO.
L-300
FILE NO.
REV. NO.



- CATENARY LIGHTING CABLES & SUSPENDED PENDANTS, SED
- (E) IN-GROUND EXISTING STREET TREES AT BLOCK 1
- 07 L-301 BRICK UNIT PAVING VEH.
- 02 L-500 GRANITE CURB
- 01 L-318 TREE GRATE
- 03 L-700 CUSTOM FIXED BENCHES BEYOND
- 02 L-301 CITY STANDARD CIP CONC. PAVING W/ METAL INLAY ON STRUCT.
- BENCH FOUNDATION BEYOND
- FIXED BOLLARD W/ CHAIN -CHAIN IS ONLY DURING NIGHT TIME. COORDINATE WITH SFMT, FOR OPERATIONAL NEEDS

01 BLOCK 1 - NORTH SECTION

3/8" = 1'-0"

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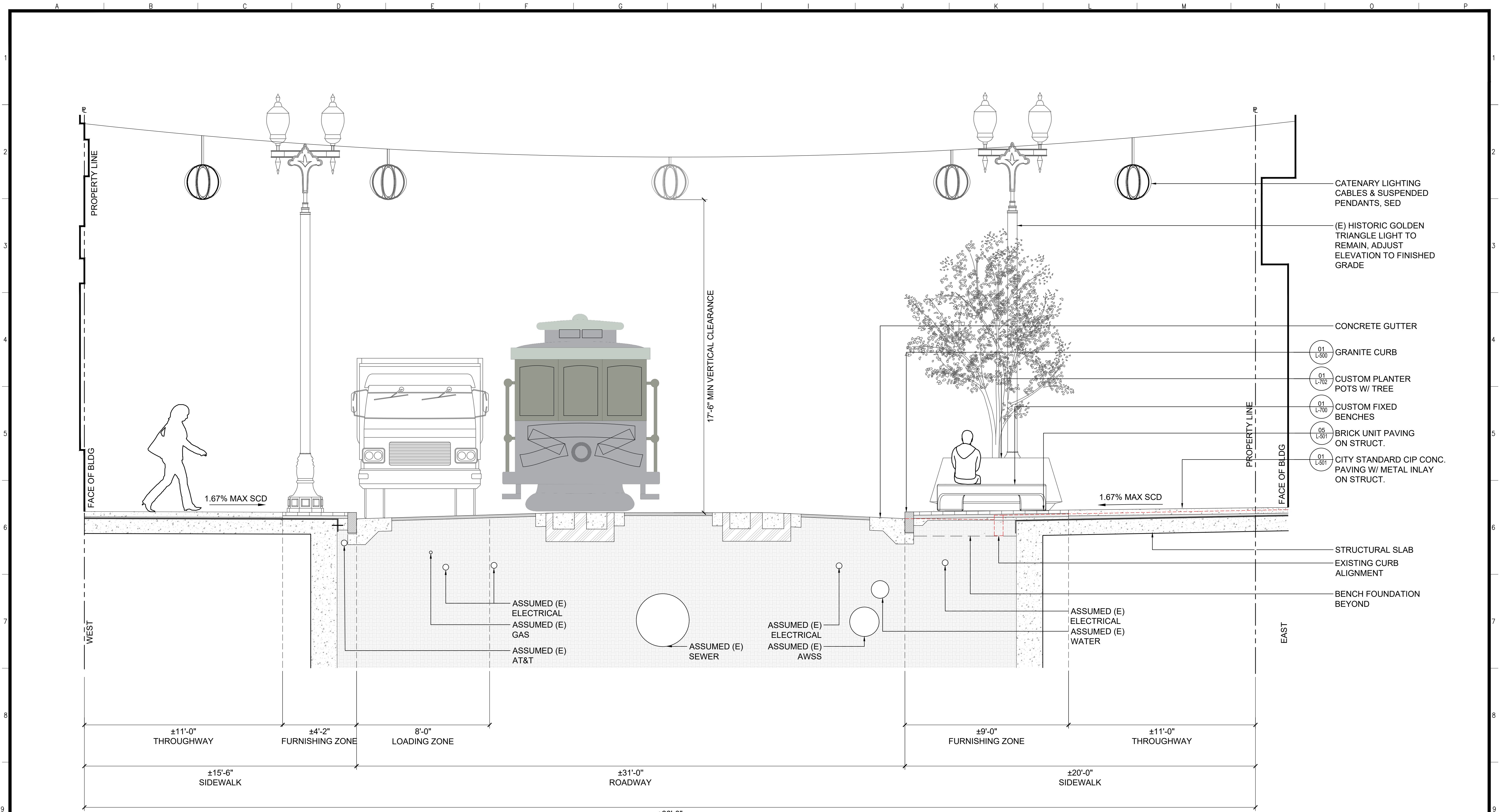
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56/170

POWELL STREETSCAPE IMPROVEMENTS PROJECT

SECTIONS AND ELEVATIONS



SOURCING EVENT ID. 000000XXXX
 DRAWING NO. **L-301**
 FILE NO.
 REV. NO.



- CATENARY LIGHTING CABLES & SUSPENDED PENDANTS, SED
- (E) HISTORIC GOLDEN TRIANGLE LIGHT TO REMAIN, ADJUST ELEVATION TO FINISHED GRADE
- CONCRETE GUTTER
- 01 L-500 GRANITE CURB
- 01 L-702 CUSTOM PLANTER POTS W/ TREE
- 01 L-700 CUSTOM FIXED BENCHES
- 05 L-301 BRICK UNIT PAVING ON STRUCT.
- 01 L-301 CITY STANDARD CIP CONC. PAVING W/ METAL INLAY ON STRUCT.
- STRUCTURAL SLAB
- EXISTING CURB ALIGNMENT
- BENCH FOUNDATION BEYOND

ASSUMED (E) ELECTRICAL
 ASSUMED (E) GAS
 ASSUMED (E) AT&T

ASSUMED (E) SEWER

ASSUMED (E) ELECTRICAL
 ASSUMED (E) AWSS

ASSUMED (E) ELECTRICAL
 ASSUMED (E) WATER

±11'-0" THROUGHWAY ±4'-2" FURNISHING ZONE 8'-0" LOADING ZONE

±15'-6" SIDEWALK ±31'-0" ROADWAY ±9'-0" FURNISHING ZONE ±11'-0" THROUGHWAY

±20'-0" SIDEWALK

01 BLOCK 2 - SOUTH SECTION ±66'-0" POWELL STREET R.O.W. 3/8" = 1'-0"

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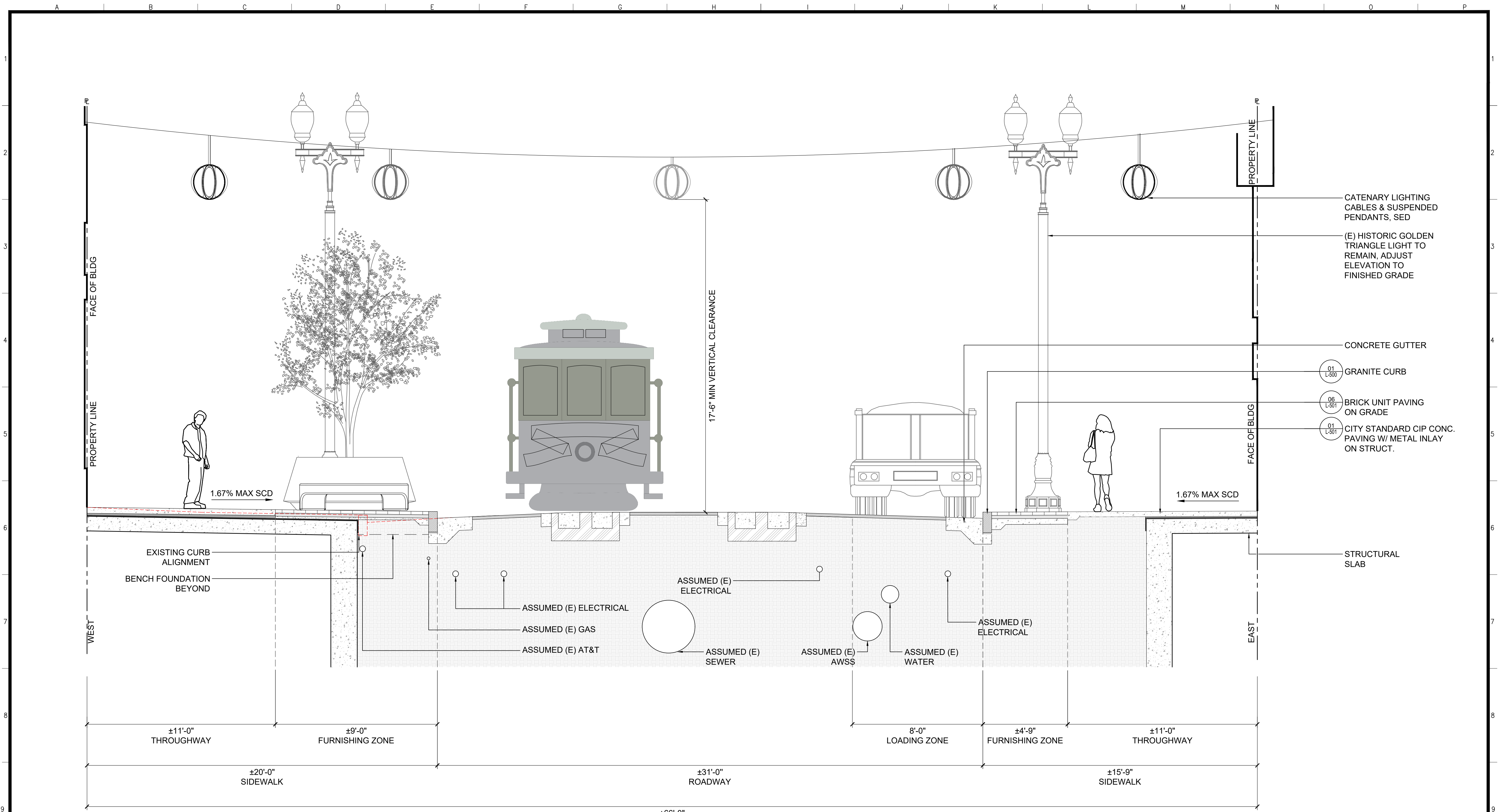
SCALE:
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 57/170

POWELL STREETSCAPE IMPROVEMENTS PROJECT

SECTIONS AND ELEVATIONS



SOURCING EVENT ID. 00000XXXX
 DRAWING NO. **L-302**
 FILE NO.
 REV. NO.



- CATENARY LIGHTING CABLES & SUSPENDED PENDANTS, SED
- (E) HISTORIC GOLDEN TRIANGLE LIGHT TO REMAIN, ADJUST ELEVATION TO FINISHED GRADE
- CONCRETE GUTTER
- 01 L-500 GRANITE CURB
- 06 L-501 BRICK UNIT PAVING ON GRADE
- 01 L-501 CITY STANDARD CIP CONC. PAVING W/ METAL INLAY ON STRUCT.

EXISTING CURB ALIGNMENT
BENCH FOUNDATION BEYOND

ASSUMED (E) ELECTRICAL
ASSUMED (E) GAS
ASSUMED (E) AT&T
ASSUMED (E) SEWER
ASSUMED (E) AWSS
ASSUMED (E) WATER

±11'-0" THROUGHWAY ±9'-0" FURNISHING ZONE 8'-0" LOADING ZONE ±4'-9" FURNISHING ZONE ±11'-0" THROUGHWAY

±20'-0" SIDEWALK ±31'-0" ROADWAY ±15'-9" SIDEWALK

±66'-0" POWELL STREET R.O.W.

01 BLOCK 2 - NORTH
SECTION

3/8" = 1'-0"

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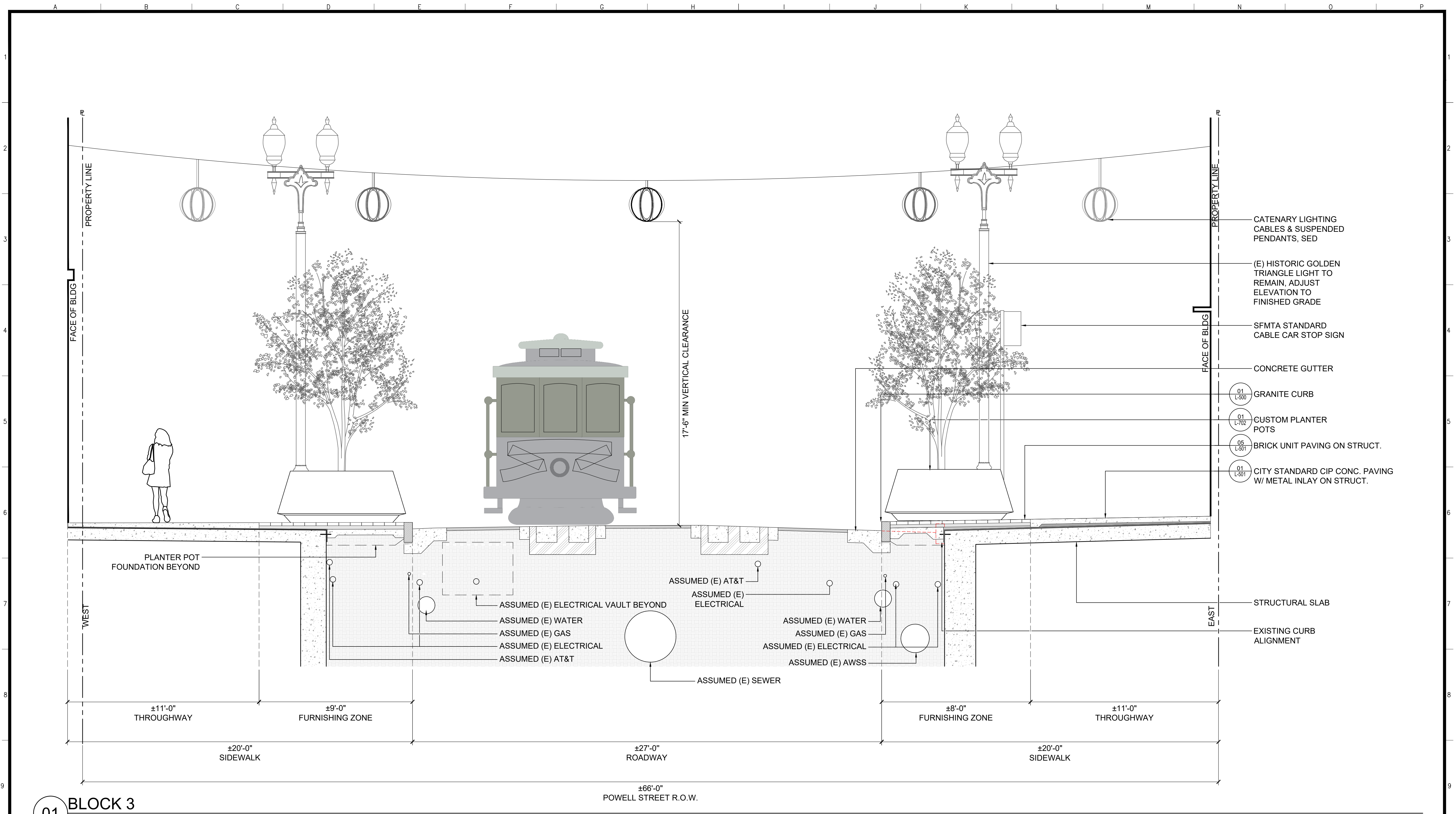
SCALE:
SHEET OF SHEETS
58/170

POWELL STREETSCAPE IMPROVEMENTS PROJECT

SECTIONS AND ELEVATIONS



SOURCING EVENT ID. 000000XXXX
DRAWING NO. **L-303**
FILE NO.
REV. NO.



- CATENARY LIGHTING CABLES & SUSPENDED PENDANTS, SED
- (E) HISTORIC GOLDEN TRIANGLE LIGHT TO REMAIN, ADJUST ELEVATION TO FINISHED GRADE
- SFMTA STANDARD CABLE CAR STOP SIGN
- CONCRETE GUTTER
- 01 L-500 GRANITE CURB
- 01 L-702 CUSTOM PLANTER POTS
- 05 L-501 BRICK UNIT PAVING ON STRUCT.
- 01 L-501 CITY STANDARD CIP CONC. PAVING W/ METAL INLAY ON STRUCT.

- STRUCTURAL SLAB
- EXISTING CURB ALIGNMENT

- ASSUMED (E) AT&T
- ASSUMED (E) ELECTRICAL VAULT BEYOND
- ASSUMED (E) WATER
- ASSUMED (E) GAS
- ASSUMED (E) ELECTRICAL
- ASSUMED (E) AT&T
- ASSUMED (E) SEWER
- ASSUMED (E) ELECTRICAL
- ASSUMED (E) WATER
- ASSUMED (E) GAS
- ASSUMED (E) ELECTRICAL
- ASSUMED (E) AWSS

PLANTER POT FOUNDATION BEYOND

±11'-0" THROUGHWAY
±20'-0" SIDEWALK

±9'-0" FURNISHING ZONE

±27'-0" ROADWAY

±8'-0" FURNISHING ZONE

±11'-0" THROUGHWAY
±20'-0" SIDEWALK

±66'-0" POWELL STREET R.O.W.

01 BLOCK 3 SECTION

3/8" = 1'-0"

NO.	DATE	DESCRIPTION	BY	APP.
TABLE OF REVISIONS				

REFERENCE INFORMATION & FILE NO. OF SURVEYS



BUREAU OF LANDSCAPE ARCHITECTURE
CITY & COUNTY OF SAN FRANCISCO
SAN FRANCISCO PUBLIC WORKS
49 SOUTH VAN NESS AVENUE, SUITE 1000
SAN FRANCISCO, CA 94103

Section Mgr:	TONY ESTERBROOKS	Date:	
Bureau Mgr:	JENNIFER COOPER	DESIGNED:	DATE:
		DRAWN:	DATE:
		CHECKED:	DATE:

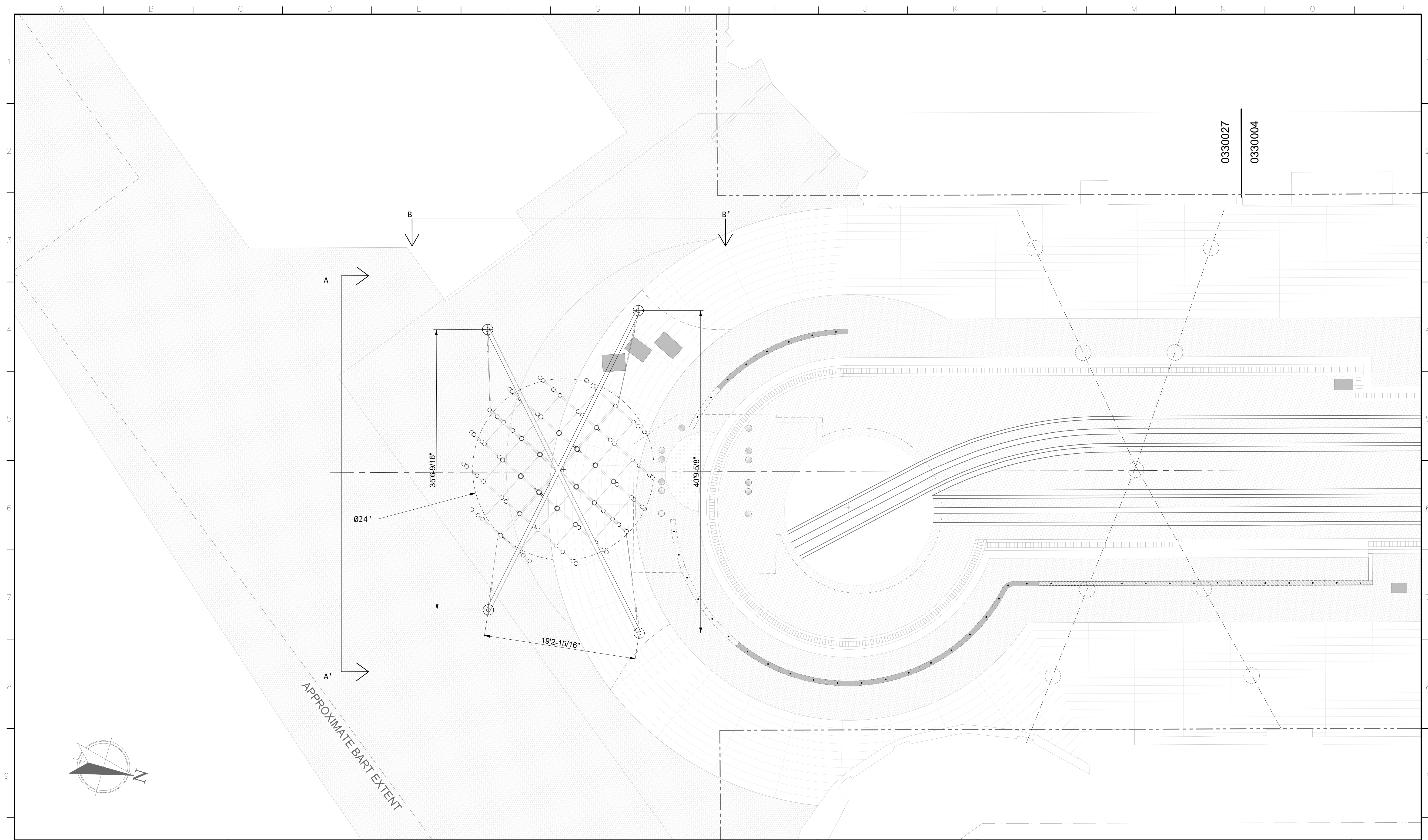
NOT FOR CONSTRUCTION

SCALE:
SHEET OF SHEETS
59/170

POWELL STREETSCAPE IMPROVEMENTS PROJECT
SECTIONS AND ELEVATIONS



SOURCING EVENT ID. 00000XXXX
DRAWING NO. **L-304**
FILE NO.
REV. NO.



NO.	DATE	DESCRIPTION	BY	APP.
TABLE OF REVISIONS THIS DRAWING WAS LAST MODIFIED: 01/29/26				

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& FILE NO. OF SURVEYS



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 CITY & COUNTY OF SAN FRANCISCO
 SAN FRANCISCO PUBLIC WORKS
 49 SOUTH VAN NESS AVENUE, SUITE 800
 SAN FRANCISCO, CA 94103

BUREAU MGR: PATRICK RIVERA

Date:
 DESIGNED: DATE: 01.29.26
 Digital Ambiance
 DRAWN: DATE: 01.29.26
 Enrique Álvarez
 CHECKED: DATE:

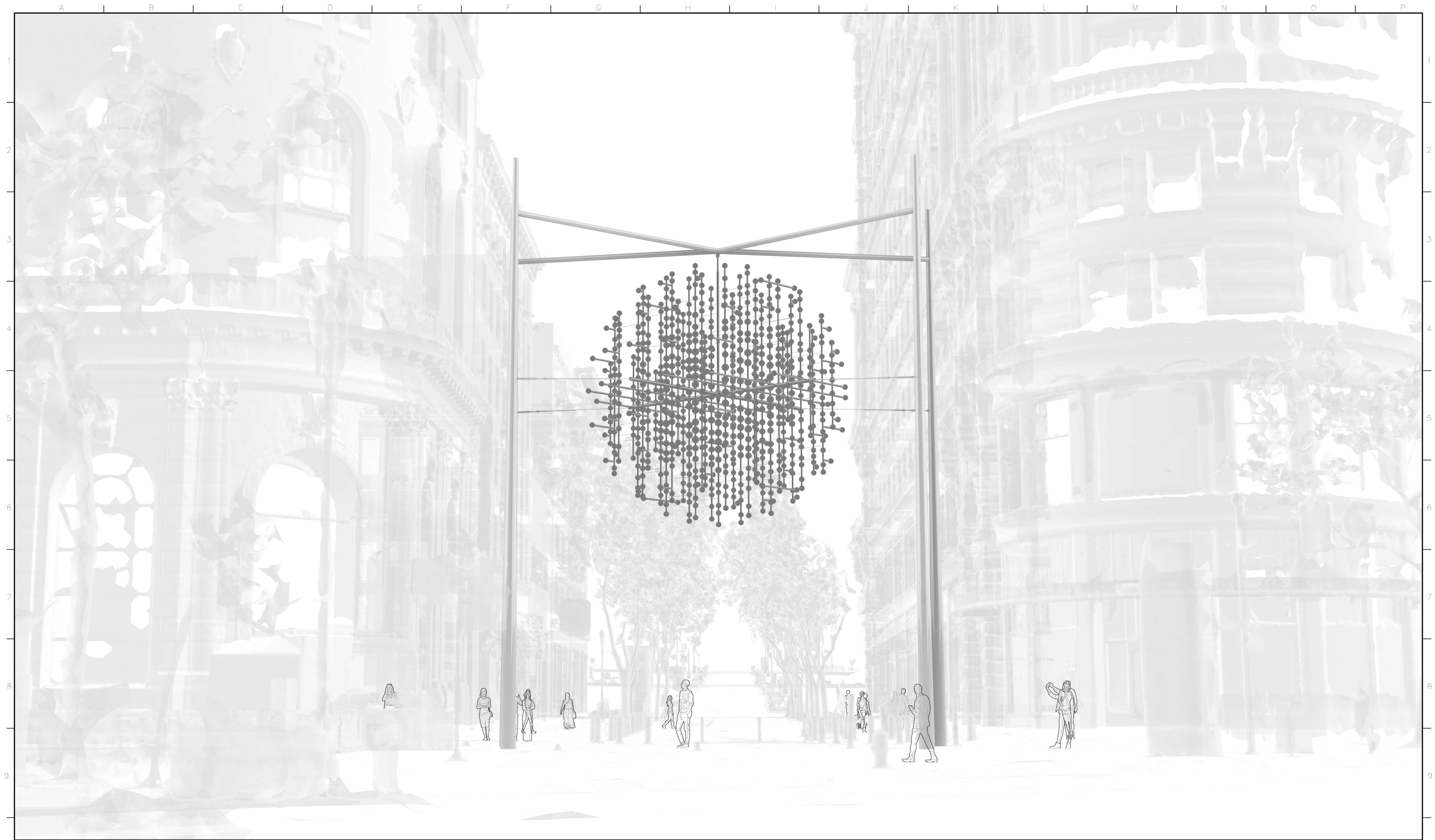
SHEET OF SHEETS
 135/170

SCALE:
 1" = 6'

POWELL STREETSCAPE IMPROVEMENTS PROJECT
 GOLDEN LANTERN PLAN



SOURCING EVENT ID:
 000000XXXX
 DRAWING NO:
 E-002
 FILE NO.
 REV. NO.



NO.	DATE	DESCRIPTION	BY	APP.
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& FILE NO. OF SURVEYS



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SAN FRANCISCO, CA 94103

BUREAU MGR:	PATRICK RIVERA
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	CHECKED: DATE:

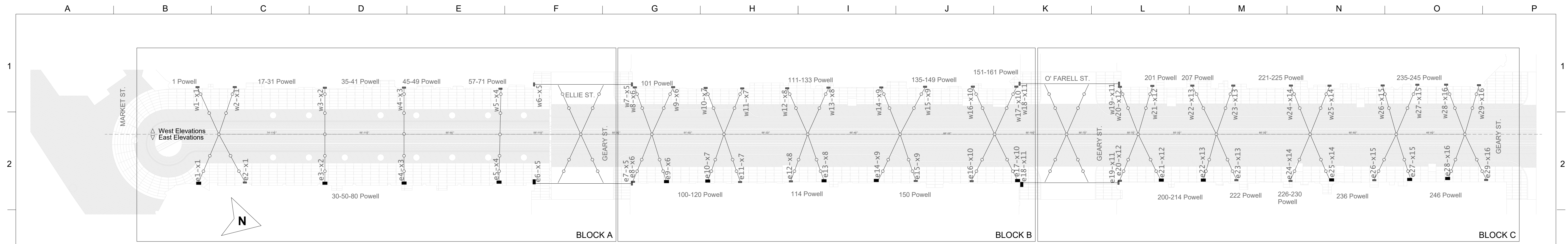
SCALE: N/A
SHEET OF SHEETS 138/170

POWELL STREETSCAPE
IMPROVEMENTS PROJECT

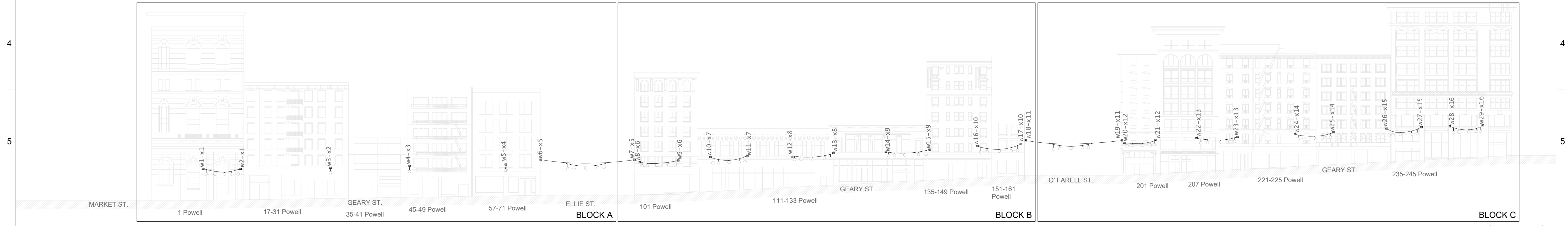
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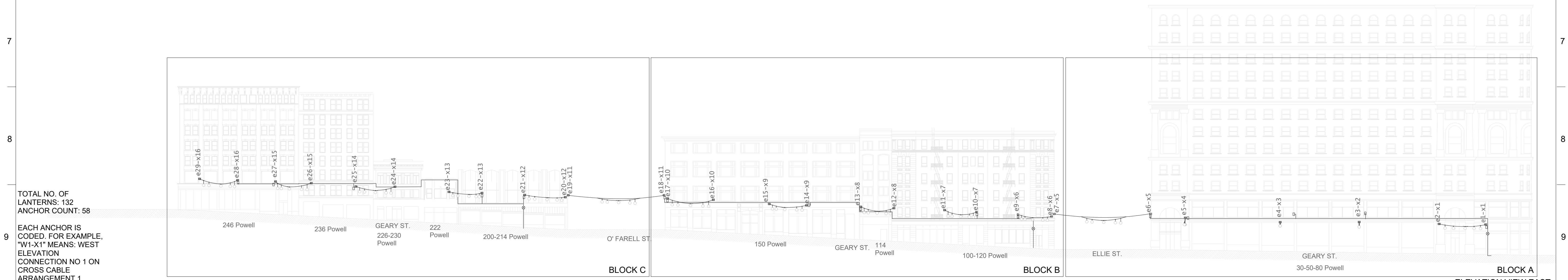
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DRAWING NO. E-005
FILE NO.
REV. NO.



PLAN VIEW



ELEVATION VIEW WEST



ELEVATION VIEW EAST

TOTAL NO. OF LANTERNS: 132
ANCHOR COUNT: 58

EACH ANCHOR IS CODED. FOR EXAMPLE, "W1-X1" MEANS: WEST ELEVATION CONNECTION NO 1 ON CROSS CABLE ARRANGEMENT 1

NO.	DATE	DESCRIPTION	BY	APP.
TABLE OF REVISIONS				
THIS DRAWING WAS LAST MODIFIED: 01/30/26				

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SAN FRANCISCO, CA 94103

BUREAU MGR: PATRICK RIVERA

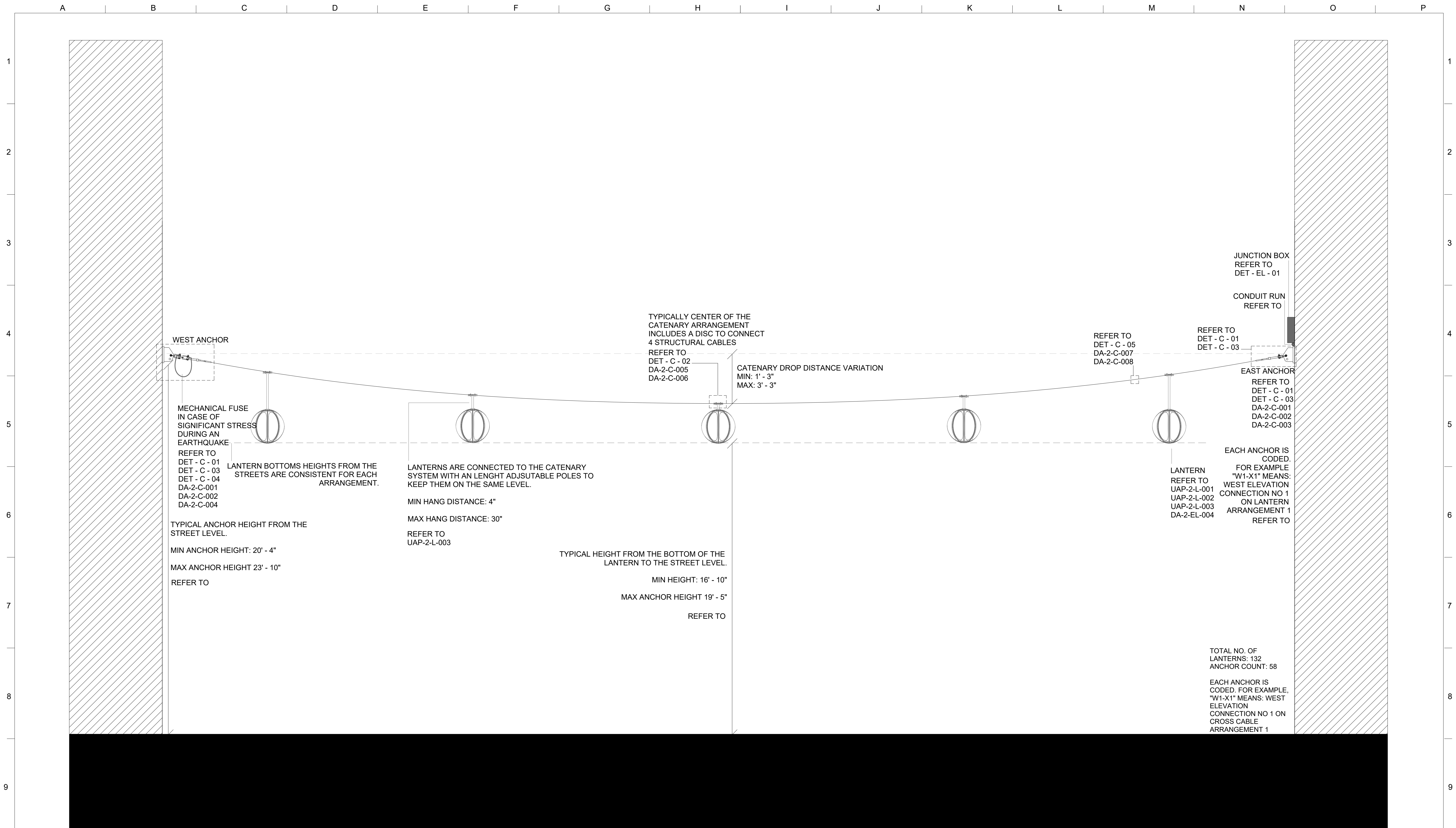
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DIGITAL AMBIANCE
DRAWN: DATE: 02.13.26
EGE DOGAN
CHECKED: DATE:

SCALE:
1" = 35'
SHEET OF SHEETS
149/170

POWELL STREETSCAPE IMPROVEMENTS PROJECT
OVERALL PLAN AND ELEVATIONS

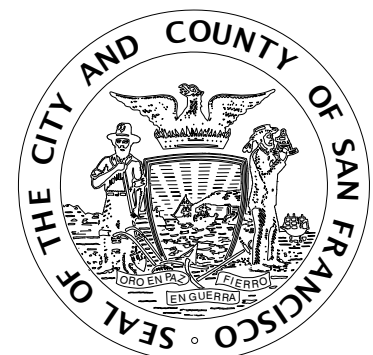


SOURCING EVENT ID. 000000XXXX
DRAWING NO. E-101
FILE NO.
REV. NO.



NO.	DATE	DESCRIPTION	BY	APP.
TABLE OF REVISIONS				
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& FILE NO. OF SURVEYS



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CITY & COUNTY OF SAN FRANCISCO
SAN FRANCISCO PUBLIC WORKS
49 SOUTH VAN NESS AVENUE, SUITE 800
SAN FRANCISCO, CA 94103

BUREAU MGR:	PATRICK RIVERA
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Date:	DESIGNED: DATE:
	DIGITAL AMBIANCE 02.13.26
	DRAWN: DATE:
	EGE DOGAN 02.13.26
	CHECKED: DATE:

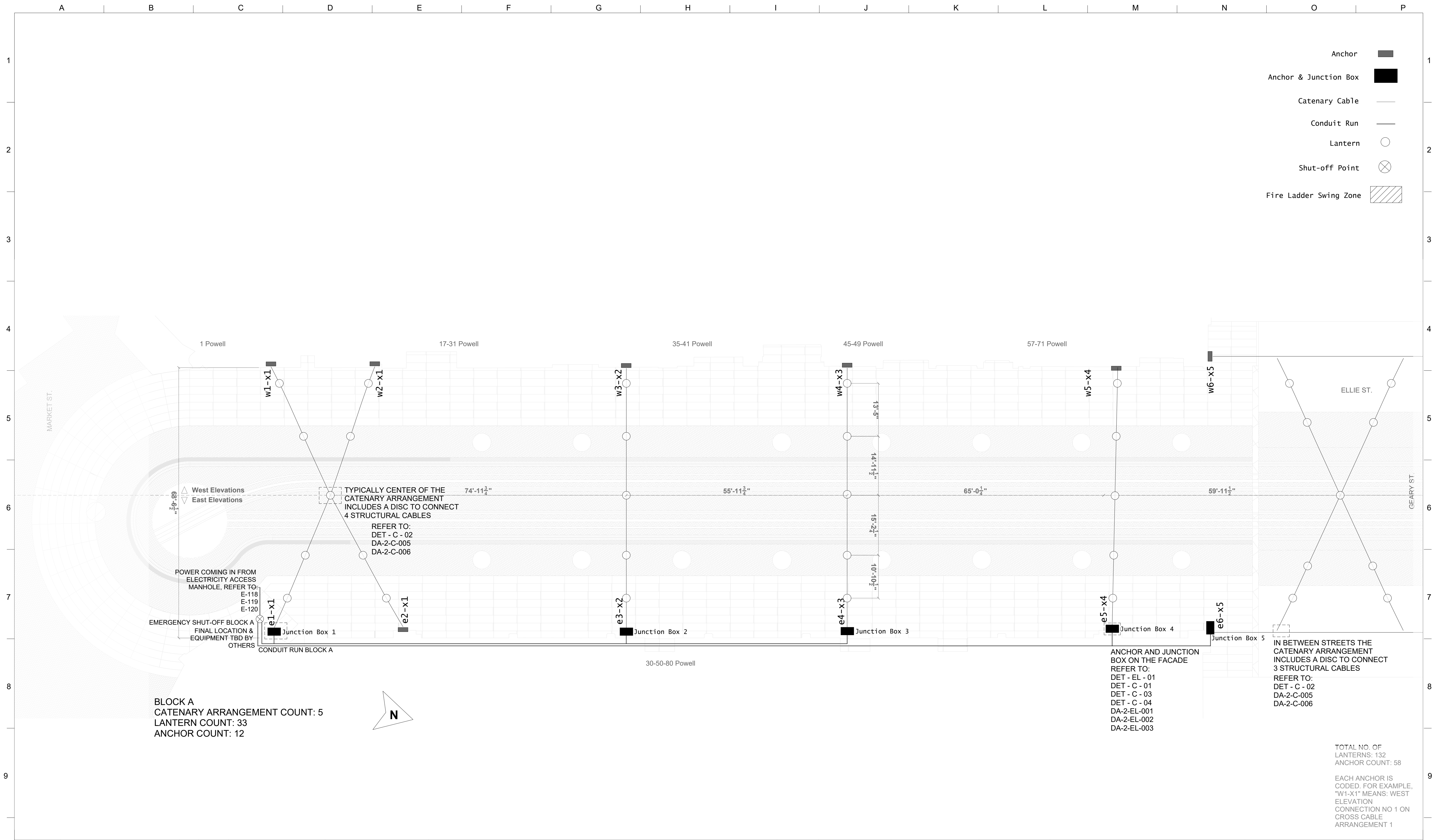
SCALE:	1" = 2.5'
SHEET OF SHEETS	150/170

POWELL STREETSCAPE
IMPROVEMENTS PROJECT

TYPICAL FRONT ELEVATION



SOURCING EVENT ID.	000000XXXX
DRAWING NO.	E-102
FILE NO.	
REV. NO.	



BLOCK A
CATENARY ARRANGEMENT COUNT: 5
LANTERN COUNT: 33
ANCHOR COUNT: 12

ANCHOR AND JUNCTION BOX ON THE FACADE REFER TO:
 DET - EL - 01
 DET - C - 01
 DET - C - 03
 DET - C - 04
 DA-2-EL-001
 DA-2-EL-002
 DA-2-EL-003

IN BETWEEN STREETS THE CATENARY ARRANGEMENT INCLUDES A DISC TO CONNECT 3 STRUCTURAL CABLES REFER TO:
 DET - C - 02
 DA-2-C-005
 DA-2-C-006

TOTAL NO. OF LANTERNS: 132
 ANCHOR COUNT: 58

EACH ANCHOR IS CODED. FOR EXAMPLE, "W1-X1" MEANS: WEST ELEVATION CONNECTION NO 1 ON CROSS CABLE ARRANGEMENT 1

NO.	DATE	DESCRIPTION	BY	APP.
TABLE OF REVISIONS				
THIS DRAWING WAS LAST MODIFIED: 01/30/26				

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 SAN FRANCISCO, CA 94103

BUREAU MGR: PATRICK RIVERA

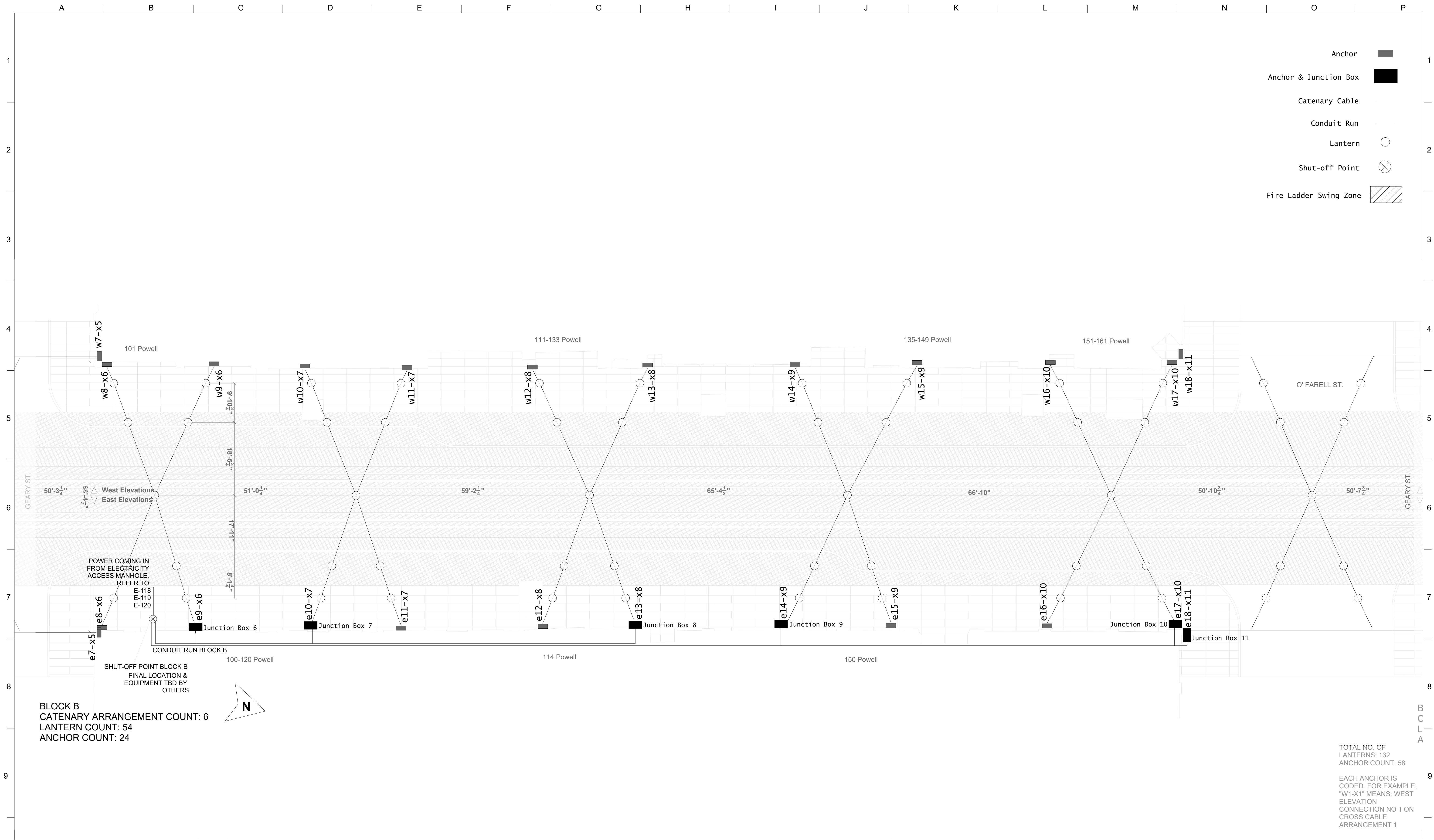
DESIGNED: DATE: 02.13.26
 DIGITAL AMBIANCE
 DRAWN: DATE: 02.13.26
 EGE DOGAN
 CHECKED: DATE:

SCALE:
1" = 12'
 SHEET OF SHEETS
 151/170

POWELL STREETSCAPE IMPROVEMENTS PROJECT
PLAN - BLOCK A



SOURCING EVENT ID. 000000XXXX
 DRAWING NO. **E-103**
 FILE NO.
 REV. NO.



BLOCK B
 CATENARY ARRANGEMENT COUNT: 6
 LANTERN COUNT: 54
 ANCHOR COUNT: 24

TOTAL NO. OF
 LANTERNS: 132
 ANCHOR COUNT: 58

EACH ANCHOR IS
 CODED. FOR EXAMPLE,
 "W1-X1" MEANS: WEST
 ELEVATION
 CONNECTION NO 1 ON
 CROSS CABLE
 ARRANGEMENT 1

NO.	DATE	DESCRIPTION	BY	APP.
TABLE OF REVISIONS				
THIS DRAWING WAS LAST MODIFIED: 01/30/26				

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 SAN FRANCISCO, CA 94103

BUREAU MGR:	PATRICK RIVERA
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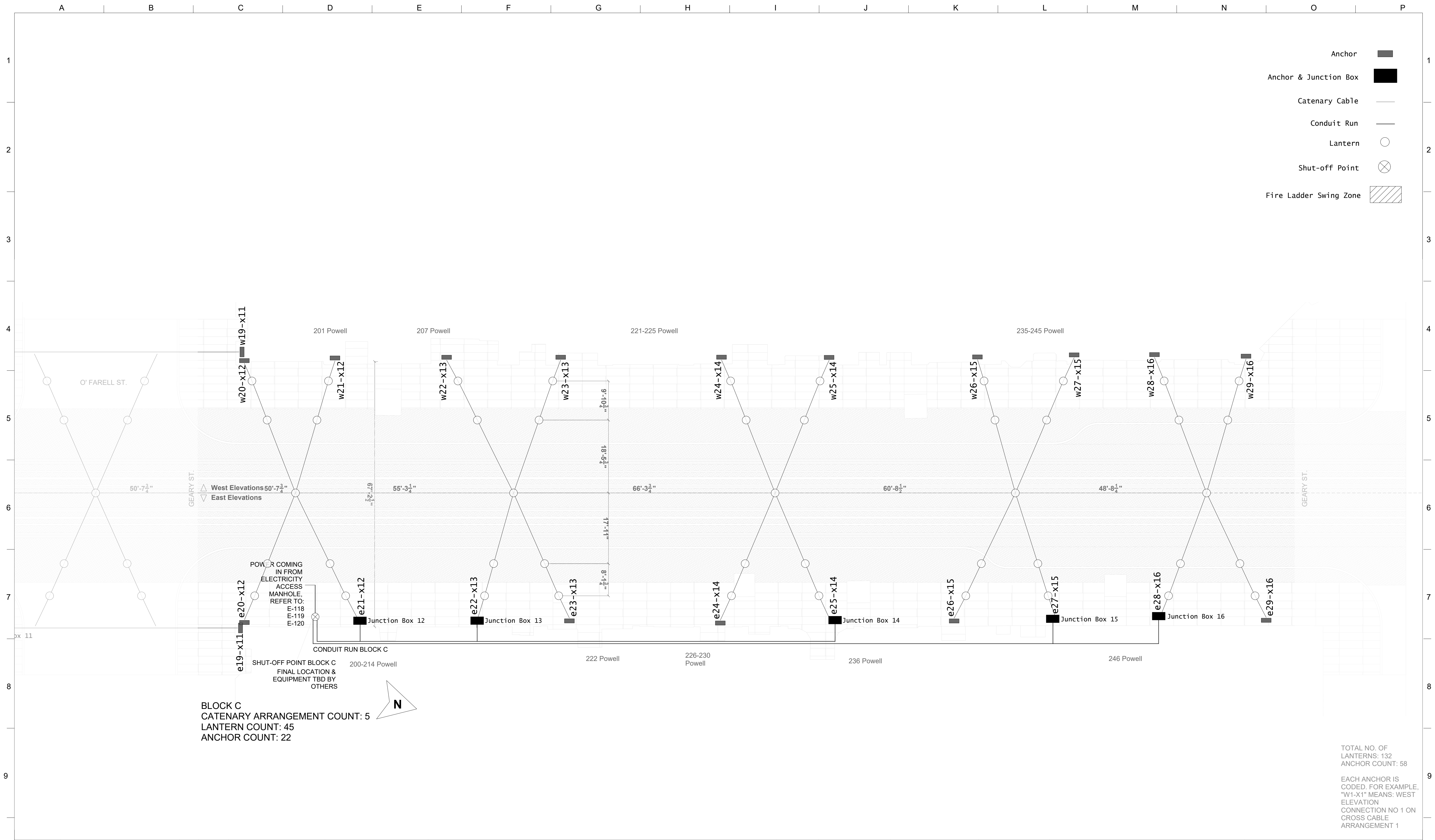
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	DRAWN: DATE:	EGE DOGAN	02.13.26
	CHECKED: DATE:		

SCALE:
 1" = 12'
 SHEET OF SHEETS
 152/170

POWELL STREETSCAPE
 IMPROVEMENTS PROJECT
 PLAN - BLOCK B



SOURCING EVENT ID.	000000XXXX
DRAWING NO.	E-104
FILE NO.	
REV. NO.	



BLOCK C
 CATENARY ARRANGEMENT COUNT: 5
 LANTERN COUNT: 45
 ANCHOR COUNT: 22

TOTAL NO. OF
 LANTERNS: 132
 ANCHOR COUNT: 58

EACH ANCHOR IS
 CODED. FOR EXAMPLE,
 "W1-X1" MEANS: WEST
 ELEVATION
 CONNECTION NO 1 ON
 CROSS CABLE
 ARRANGEMENT 1

NO.	DATE	DESCRIPTION	BY	APP.
TABLE OF REVISIONS				
THIS DRAWING WAS LAST MODIFIED: 01/30/26				

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BUREAU MGR:	PATRICK RIVERA
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Date:	DESIGNED: DATE: DIGITAL AMBIANCE 02.13.26
	DRAWN: DATE: EGE DOGAN 02.13.26
	CHECKED: DATE:

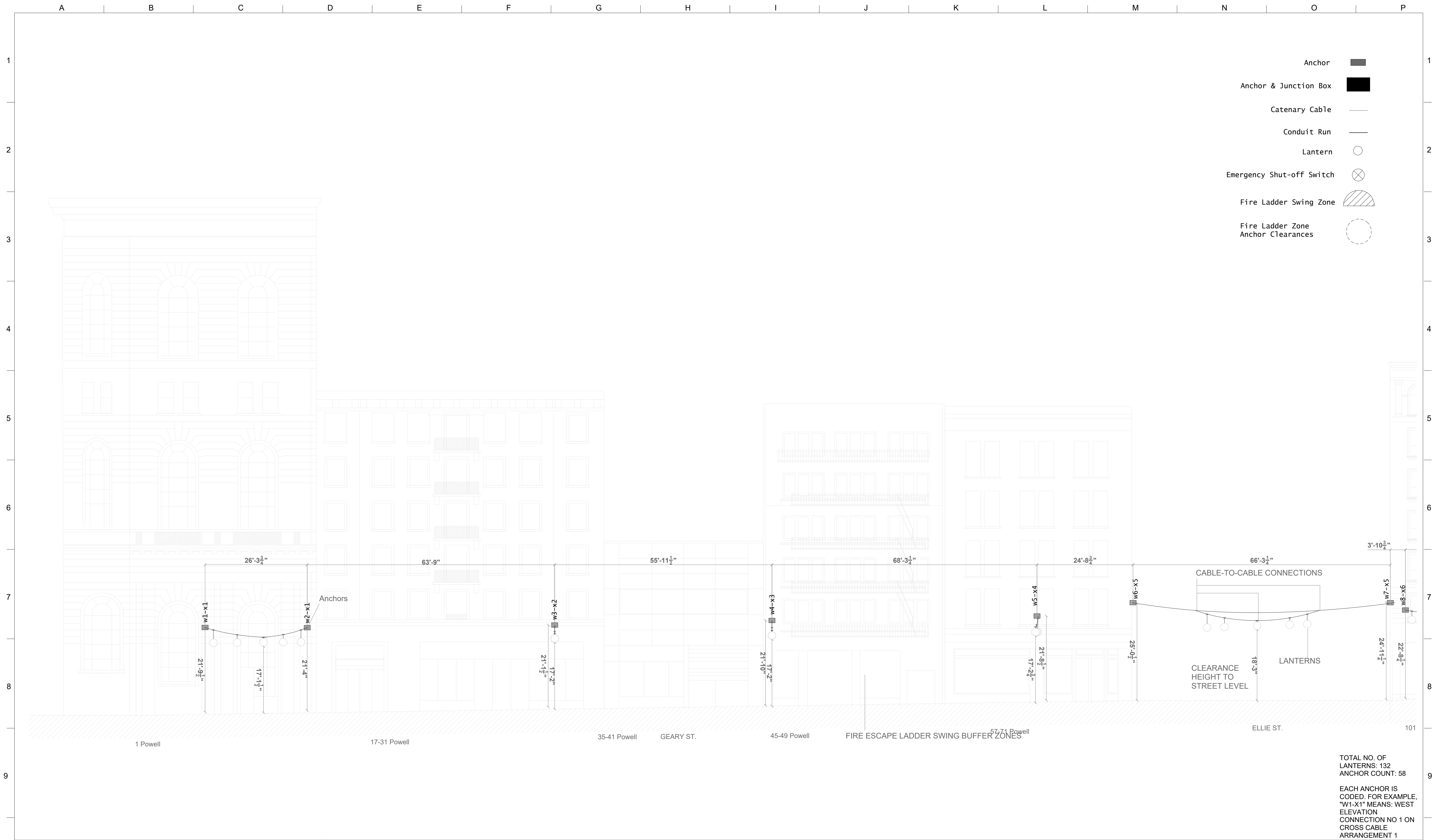
SCALE:	1" = 12'
SHEET OF SHEETS	153/170

POWELL STREETSCAPE
 IMPROVEMENTS PROJECT

PLAN - BLOCK C



SOURCING EVENT ID. 000000XXXX
DRAWING NO. E-105
FILE NO.
REV. NO.



- Anchor
- Anchor & Junction Box
- Catenary Cable
- Conduit Run
- Lantern
- Emergency Shut-off Switch
- Fire Ladder Swing Zone
- Fire Ladder Zone Anchor Clearances

TOTAL NO. OF LANTERNS: 132
ANCHOR COUNT: 58

EACH ANCHOR IS CODED. FOR EXAMPLE, "W1-X1" MEANS: WEST ELEVATION CONNECTION NO 1 ON CROSS CABLE ARRANGEMENT 1

NO.	DATE	DESCRIPTION	BY	APP.
TABLE OF REVISIONS				
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BUREAU MGR:	PATRICK RIVERA
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DESIGNED:	DATE:
DIGITAL AMBIANCE	02.13.26
DRAWN:	DATE:
EGE DOGAN	02.13.26
CHECKED:	DATE:

Date:

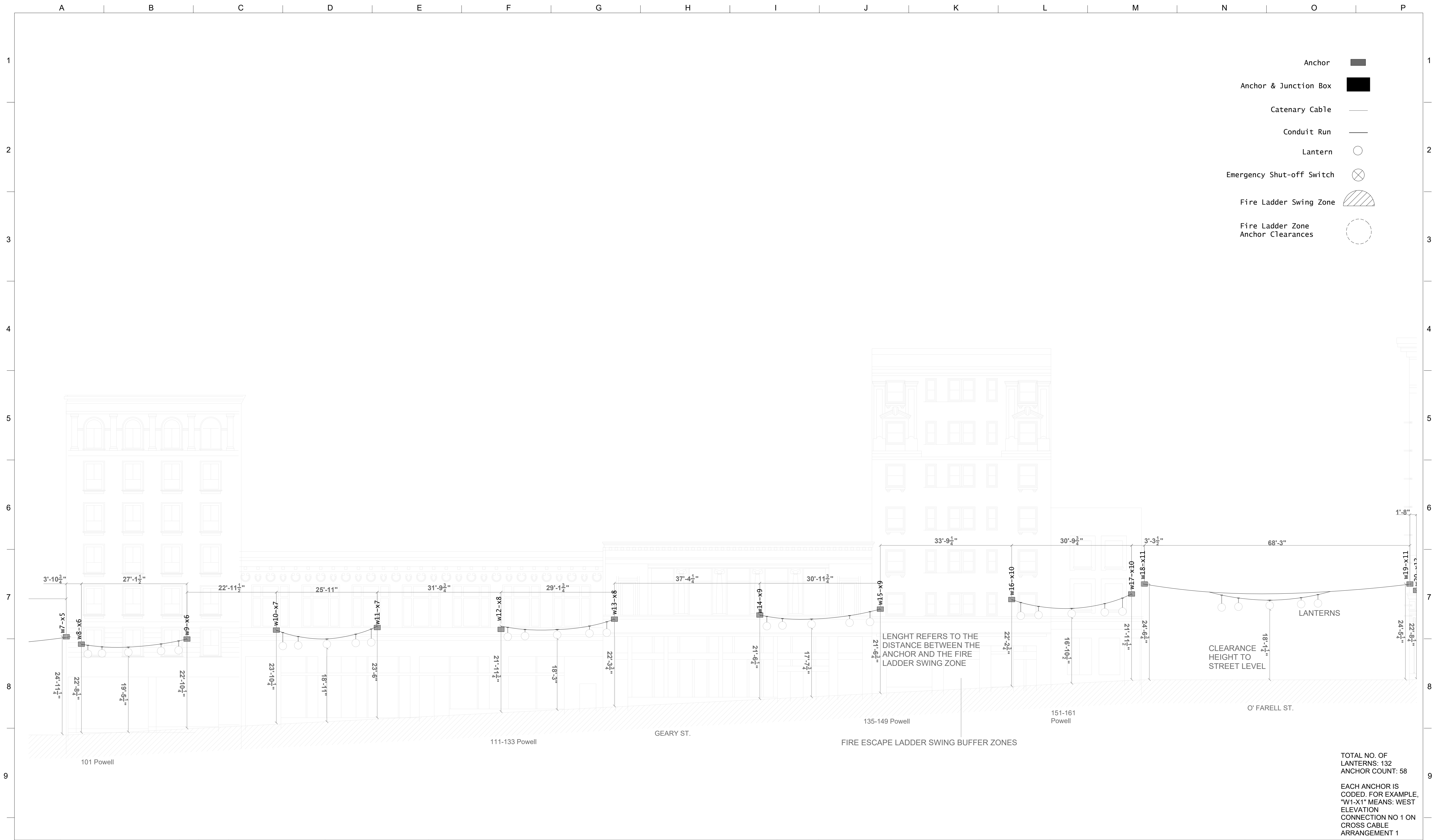
SCALE:	1" = 12'
SHEET OF SHEETS	154/170

POWELL STREETSCAPE IMPROVEMENTS PROJECT

WEST ELEVATION BLOCK A



SOURCING EVENT ID.	000000XXXX
DRAWING NO.	E-106
FILE NO.	
REV. NO.	



TOTAL NO. OF LANTERNS: 132
 ANCHOR COUNT: 58

EACH ANCHOR IS CODED. FOR EXAMPLE, "W1-X1" MEANS: WEST ELEVATION CONNECTION NO 1 ON CROSS CABLE ARRANGEMENT 1

NO.	DATE	DESCRIPTION	BY	APP.
TABLE OF REVISIONS				
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 SAN FRANCISCO, CA 94103

BUREAU MGR:	PATRICK RIVERA
Date:	

DESIGNED:	DATE:
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DRAWN:	DATE:
EGE DOGAN	02.13.26
CHECKED:	DATE:

SCALE:
 1" = 12'

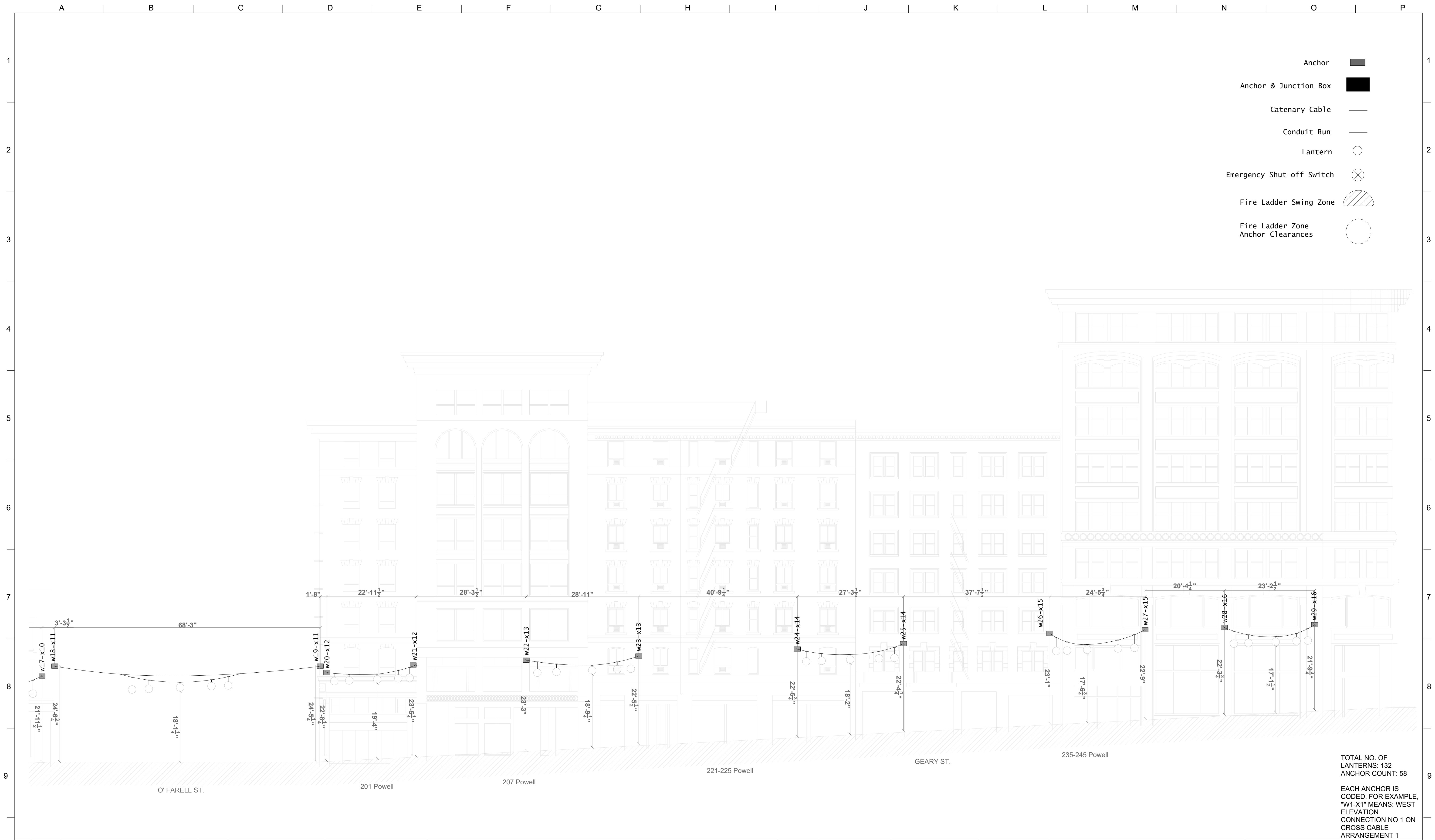
SHEET OF SHEETS
 155/170

POWELL STREETSCAPE IMPROVEMENTS PROJECT

WEST ELEVATION BLOCK B



SOURCING EVENT ID.	000000XXXX
DRAWING NO.	E-107
FILE NO.	
REV. NO.	



TOTAL NO. OF LANTERNS: 132
ANCHOR COUNT: 58

EACH ANCHOR IS CODED. FOR EXAMPLE, "W1-X1" MEANS: WEST ELEVATION CONNECTION NO 1 ON CROSS CABLE ARRANGEMENT 1

NO.	DATE	DESCRIPTION	BY	APP.
TABLE OF REVISIONS				
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SAN FRANCISCO, CA 94103

BUREAU MGR: PATRICK RIVERA

Date:
DESIGNED: DATE: 02.13.26
DIGITAL AMBIANCE
DRAWN: DATE: 02.13.26
EGE DOGAN
CHECKED: DATE:

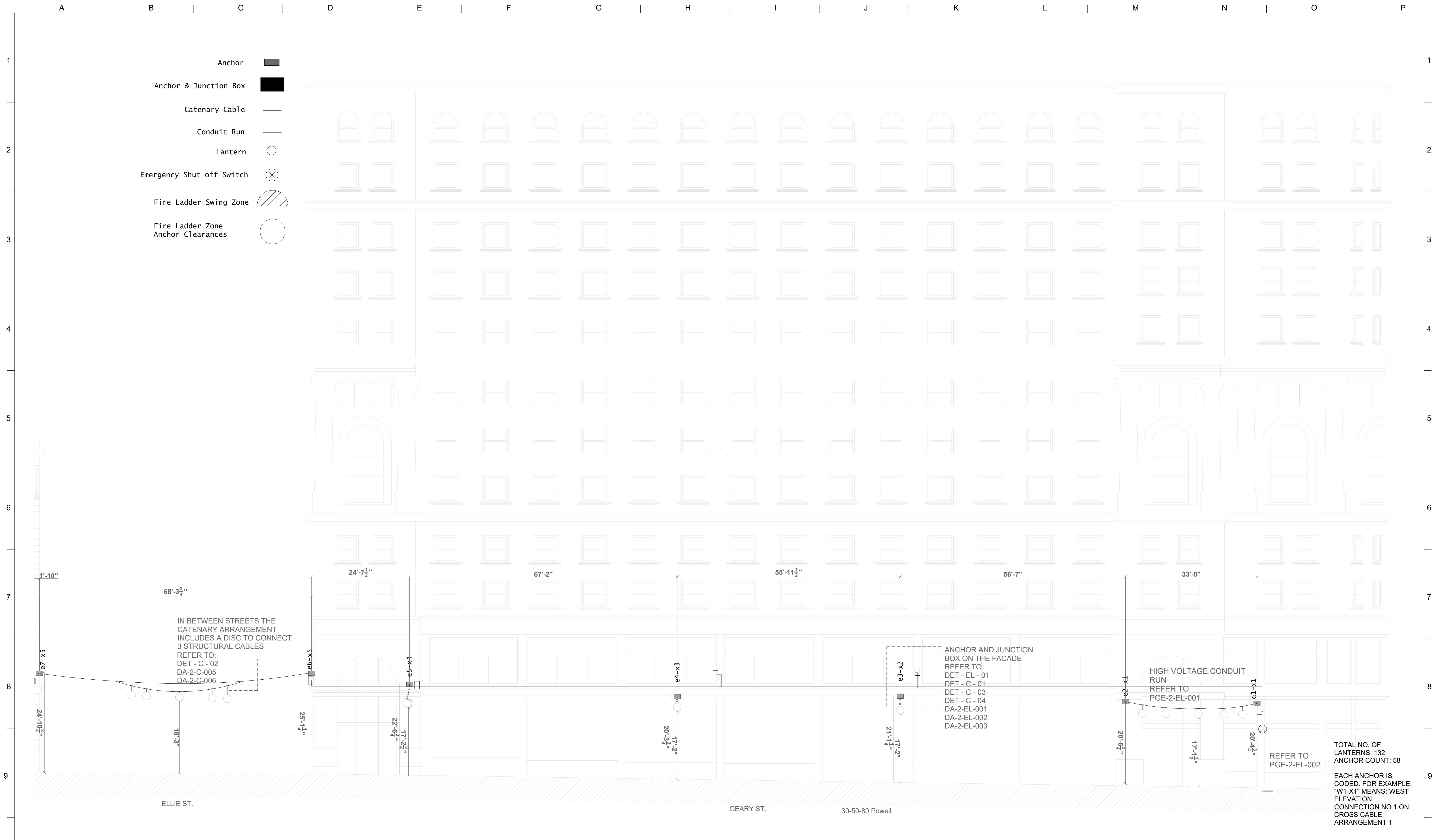
SCALE:
1" = 12'

SHEET OF SHEETS
156/170

POWELL STREETSCAPE IMPROVEMENTS PROJECT
WEST ELEVATION BLOCK C



SOURCING EVENT ID: 00000XXXX
DRAWING NO: **E-108**
FILE NO:
REV. NO:



NO.	DATE	DESCRIPTION	BY	APP.
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 SAN FRANCISCO, CA 94103

BUREAU MGR: PATRICK RIVERA

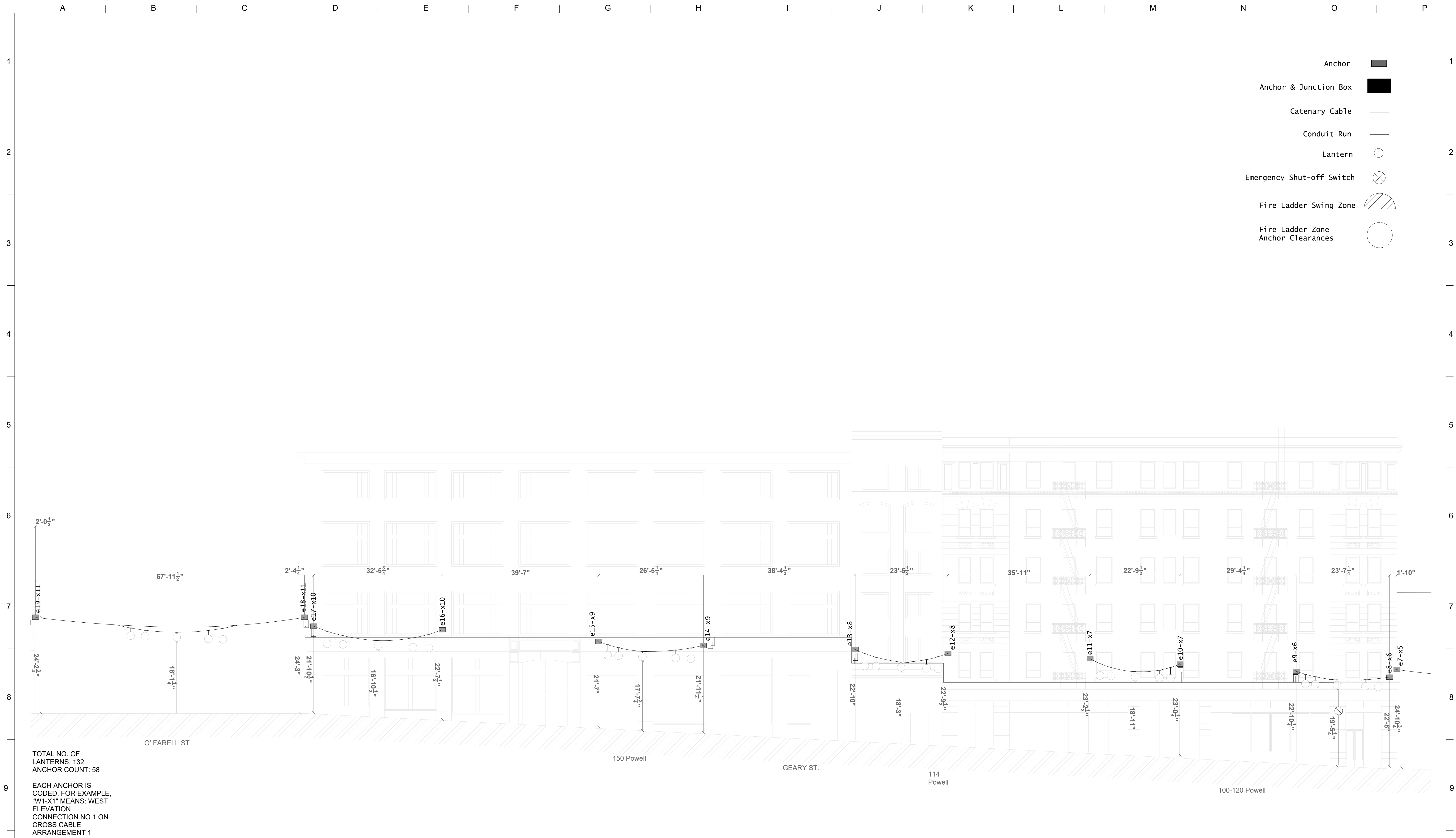
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 DIGITAL AMBIANCE
 DRAWN: DATE: 02.13.26
 EGE DOGAN
 CHECKED: DATE:

SCALE:
 1" = 12'
 SHEET OF SHEETS
 157/170

POWELL STREETSCAPE IMPROVEMENTS PROJECT
EAST ELEVATION BLOCK A



SOURCING EVENT ID: 000000XXXX
 DRAWING NO: E-109
 FILE NO.
 REV. NO.



TOTAL NO. OF LANTERNS: 132
ANCHOR COUNT: 58

EACH ANCHOR IS CODED. FOR EXAMPLE, "W1-X1" MEANS: WEST ELEVATION CONNECTION NO 1 ON CROSS CABLE ARRANGEMENT 1

NO.	DATE	DESCRIPTION	BY	APP.
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BUREAU MGR: PATRICK RIVERA

Date: DESIGNED: DATE: 02.13.26
DIGITAL AMBIANCE
DRAWN: DATE: 02.13.26
EGE DOGAN
CHECKED: DATE:

SCALE: 1" = 12'

SHEET OF SHEETS
158/170









POWELL STREETSCAPE IMPROVEMENTS PROJECT
EAST ELEVATION BLOCK B



SOURCING EVENT ID. 000000XXXX
DRAWING NO. E-110
FILE NO.
REV. NO.

A B C D E F G H I J K L M N O P

1

- Anchor 
- Anchor & Junction Box 
- Catenary Cable 
- Conduit Run 
- Lantern 
- Emergency Shut-off Switch 
- Fire Ladder Swing Zone 
- Fire Ladder Zone Anchor Clearances 

2

3

4

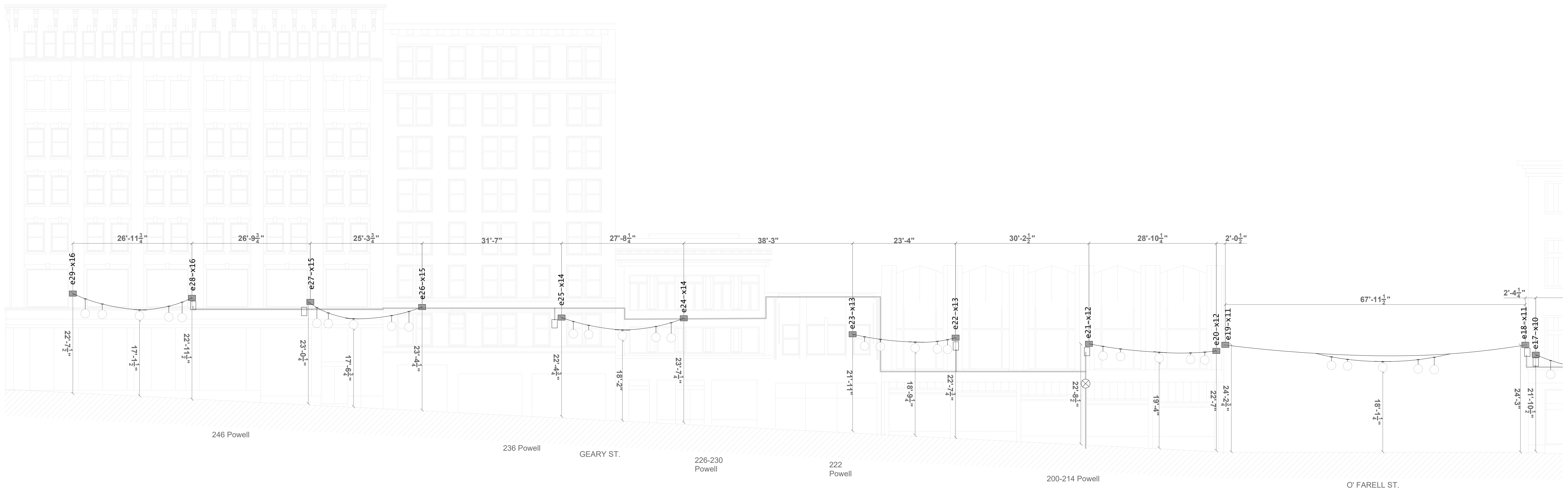
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6

7

8

9



TOTAL NO. OF LANTERNS: 132
ANCHOR COUNT: 58

EACH ANCHOR IS CODED. FOR EXAMPLE, "W1-X1" MEANS: WEST ELEVATION CONNECTION NO 1 ON CROSS CABLE ARRANGEMENT 1

NO.	DATE	DESCRIPTION	BY	APP.
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SAN FRANCISCO, CA 94103

BUREAU MGR: PATRICK RIVERA

Date: DESIGNED: DATE: DIGITAL AMBIANCE 02.13.26
DRAWN: DATE: EGE DOGAN 02.13.26
CHECKED: DATE:

SCALE: 1" = 12'

SHEET OF SHEETS 159/170

POWELL STREETSCAPE IMPROVEMENTS PROJECT
EAST ELEVATION BLOCK C



SOURCING EVENT ID. 000000XXXX
DRAWING NO. E-111
FILE NO.
REV. NO.

Lam, Samuel

From: Woo, Michelle (DPW) <Michelle.Woo@sfdpw.org>
Sent: Tuesday, March 17, 2026 10:45 AM
To: Lam, Samuel
Cc: Chong, Victoria; K. Veronica Rivera; Xiaoye Zhang; Calleros, Deanna (DPW); Yuan, Stephanie (DPW)
Subject: FW: Powell Streetscape - SFFD Follow-Up

EXT

Hello Sam –

Please see email below from Captain O’Keeffe / SFFD providing approval of the turning templates. If there may be additional info needed, please let us know.

Thanks!



Michelle Woo
Project Manager

San Francisco Public Works | Project Delivery Division | Bureau of Project Management
49 South Van Ness Avenue, Suite 700 | San Francisco, CA 94103 | (628) 271-2155

From: O’Keeffe, Tracy (FIR) <tracy.o’keeffe@sfgov.org>
Sent: Monday, March 16, 2026 2:24 PM
To: K. Veronica Rivera <vrivera@fieldoperations.net>; Woo, Michelle (DPW) <Michelle.Woo@sfdpw.org>
Subject: Re: Powell Streetscape - SFFD Follow-Up

The turning templates are good. Thank you for providing.

Captain Tracy O’Keeffe

From: K. Veronica Rivera <vrivera@fieldoperations.net>
Sent: Monday, March 16, 2026 1:53:43 PM
To: O’Keeffe, Tracy (FIR) <tracy.o’keeffe@sfgov.org>; Woo, Michelle (DPW) <Michelle.Woo@sfdpw.org>
Subject: RE: Powell Streetscape - SFFD Follow-Up

Thank you so much Captain!

Please let us know if you have any question or comments on the turning radii for the fire truck. They are all good form our side, but we wanted to get your blessing on them as well.

Best,
Veronica

K. Veronica Rivera, ASLA, PLA
Associate Partner

fieldoperations

649 Front Street
San Francisco, CA 94111
415 943 9197 x153
(787) 632-1309
vriviera@fieldoperations.net
www.fieldoperations.net

Our new website is now live. Check out our new look [here](#).

From: O'Keeffe, Tracy (FIR) <tracy.o'keeffe@sfgov.org>

Sent: Monday, March 16, 2026 1:45 PM

To: K. Veronica Rivera <vriviera@fieldoperations.net>; Woo, Michelle (DPW) <Michelle.Woo@sfdpw.org>

Subject: Re: Powell Streetscape - SFFD Follow-Up

Dear Powell Street Improvement Team,

Thank you for the follow-up and for sharing the presentation summarizing our recent discussion. Based on our call and the materials provided, the San Francisco Fire Department is comfortable proceeding with the design assumptions as outlined below:

Design Assumptions:

- Catenary lighting will maintain a typical spacing of 50 feet between catenaries along the center of the roadway.
- Blocks 2 and 3 will maintain a 26-foot fire lane.
- Block 1 will maintain a 20-foot fire lane, due to existing tree constraints. The block will remain flush, with trapezoidal pavers delineating the cable car lanes.
- Catenary light cables will be installed between 20–22 feet above finished grade (primarily 22 feet) to maintain adequate clearance for ladder truck access and maneuverability.
- Cables will remain outside a 24-foot clearance circle centered at each fire escape, with a 5-foot deep clearance zone directly in front of each fire escape.
- The catenary light spheres may, in limited instances, fall within the 24-inch wood ladder zone. Based on the description provided, the flexible connection of the spheres to the cable is assumed to allow sufficient movement if contacted by a ladder.
- Some 24-foot wood ladder clearance zones may overlap due to the proximity of adjacent fire escapes.
- One emergency shut-off per block is assumed.

Follow-Up Items:

Design Team:

- The design team will continue evaluating viable locations for the emergency shut-off controls.
- The design team will share progress on anchoring strategies to building facades, including considerations should cables need to be detached during an emergency.
- Invitation to participate in the site visit during the mock-up phase.

SFFD:

- Provide the key for the shut-off access box.

Based on the information reviewed and our discussion, you may proceed with the design development using the parameters described above. Please reach out if additional questions arise or if further review is needed as the design progresses.

Thank you again for the collaborative approach in working through the various conditions along Powell Street.

Captain Tracy O'Keefe

From: K. Veronica Rivera <vrivera@fieldoperations.net>

Sent: Thursday, March 5, 2026 10:17 AM

To: O'Keefe, Tracy (FIR) <tracy.o'keefe@sfgov.org>

Cc: Woo, Michelle (DPW) <Michelle.Woo@sfdpw.org>; Xiaoye Zhang <xzhang@fieldoperations.net>

Subject: RE: Powell Streetscape - SFFD Follow-Up

Hi Captain, great to hear –

See below email/letter written in your voice LOL. You can send this through email back to be and/or Michelle. That will suffice for the next steps. In addition to the catenary, we also wanted to get your eyes on the **truck turning templates for Powell St**. See attached. As you can see in green, the fire truck can easily make the turns @ each of the proposed corner radii. Happy to jump on a call to discuss if you have any questions or comments in this regard.

As a treat of the mockup test that are ongoing, see attached a little video shared by our consultant.

Dear Powell St Improvement team,

Thank you for the follow-up and for sharing the presentation summarizing our recent discussion. Based on our call and the materials provided, the Fire Department is comfortable proceeding with the design assumptions as outlined below:

Design Assumptions

- Catenary lighting will maintain a typical spacing of 50 feet between catenaries along the center of the roadway.
- Blocks 2 and 3 will maintain a 26-foot fire lane.
- Block 1 will maintain a 20-foot fire lane, due to existing tree constraints. The block will remain flush, with trapezoidal pavers delineating the cable car lanes.
- Catenary light cables will be installed between 20–22 feet above finished grade (primarily 22 feet) to maintain adequate clearance for ladder truck access and maneuverability.
- Cables will remain outside a 24-foot clearance circle centered at each fire escape, with a 5-foot deep clearance zone directly in front of each fire escape.

SFFD Review

- The catenary light spheres may, in limited instances, fall within the 24-inch wood ladder zone. Based on the description provided, the flexible connection of the spheres to the cable is assumed to allow sufficient movement if contacted by a ladder.
- Some 24-foot wood ladder clearance zones may overlap due to the proximity of adjacent fire escapes.
- One emergency shut-off per block is assumed.

Follow-Up Items:

Design Team:

- The design team will continue evaluating viable locations for the emergency shut-off controls.
- The design team will share progress on anchoring strategies to building facades, including considerations should cables need to be detached during an emergency.
- Invitation to participate in the site visit during the mock-up phase.

SFFD:

- Provide the specifications for the shut-off access box key.

Based on the information reviewed and our discussion, you may proceed with the design development using the parameters described above. Please reach out if additional questions arise or if further review is needed as the design progresses.

Thank you again for the collaborative approach in working through the various conditions along Powell Street.

Sincerely,
Captain Trace O'Keeffe
San Francisco Fire Department

Best,
Veronica

K. Veronica Rivera, ASLA, PLA
Associate Partner

fieldoperations

649 Front Street
San Francisco, CA 94111
415 943 9197 x153
(787) 632-1309
vriviera@fieldoperations.net
www.fieldoperations.net

From: O'Keeffe, Tracy (FIR) <tracy.o'keeffe@sfgov.org>
Sent: Wednesday, February 25, 2026 6:53 AM
To: K. Veronica Rivera <vriviera@fieldoperations.net>
Subject: RE: Powell Streetscape - SFFD Follow-Up

Veronica,



ABBREVIATED CEQA CHECKLIST FOR Better Streets Plan Improvement Projects

Please include the following supporting materials with this checklist:

- Project Description and scope of work
- Existing and Proposed Site plans
- Site photos
- Scope of work for: Air Quality Analysis Tech Memo (if applicable)¹
- Green House Gas Emission Checklist² (if applicable)

I - PROJECT INFORMATION	
DATE	
PROJECT NAME	
LOCATION/ NEIGHBORHOOD	
CONSTRUCTION DURATION	
II - PROJECT CONTACT	
RESPONSIBLE AGENCY	
NAME	
ADDRESS	
PHONE	
EMAIL	
III - PROJECT CHARACTERISTICS	
STREET TYPE ³	<input type="checkbox"/> Varies (See attachment _____) OR Provide a description:
STREET NAME	
⁴ FROM (CROSS-STREET 1) TO (CROSS-STREET 2)	

¹ Individual projects prepared pursuant to the BSP would be required to undergo a separate environmental review that would consider whether the Proposed Project's location and construction plan could affect nearby sensitive receptors - p. 123 of the BSP's PMND - [Contact EP planner for a copy of scope of work outline].

² Individual streetscape projects would be required to undergo a separate environmental review pursuant to CEQA. The environmental review would include an analysis of the individual project's potential to emit GHGs. p.128 of the BSP's PMND. [Contact EP planner for a copy of GHG Checklist].

³ See Table 1 in PMND and verify final list of street types with the online version of the BSP.

⁴ Street type determines what elements are appropriate for a design element. Different blocks of the same street may be characterized as different street types pursuant to BSP. Therefore, need to provide boundaries for project segments.

PROJECT NAME:

PROJECT SCREENING PART I

(On the table below, please identify BSP's design elements that are part of the proposed project)

DETAILED DESIGNED ELEMENTS

STANDARD IMPROVEMENTS

BSP NUMBER/ NAME	PROJECT ELEMENT	Requires Subsequent Environmental Review⁵ (EP PLANNER DETERMINATION ONLY)
SI-1 Accessible curb ramps	<input type="checkbox"/>	<input type="checkbox"/>
SI-2 Marked crosswalks	<input type="checkbox"/>	<input type="checkbox"/>
SI-3 Pedestrian signal timing	<input type="checkbox"/>	<input type="checkbox"/>
SI-4 Curb radii guidelines	<input type="checkbox"/>	<input type="checkbox"/>
SI-5 Corner curb extensions	<input type="checkbox"/>	<input type="checkbox"/>
SI-6 Street trees	<input type="checkbox"/>	<input type="checkbox"/>
SI-7 Tree basin furnishing	<input type="checkbox"/>	<input type="checkbox"/>
SI-8 Sidewalk planters	<input type="checkbox"/>	<input type="checkbox"/>
SI-9 Stormwater management tools	<input type="checkbox"/>	<input type="checkbox"/>
SI-10 Street lighting	<input type="checkbox"/>	<input type="checkbox"/>
SI-11 Special paving	<input type="checkbox"/>	<input type="checkbox"/>
SI-12 Site furnishings	<input type="checkbox"/>	<input type="checkbox"/>

CASE-BY-CASE IMPROVEMENTS

CBC-1 High-visibility crosswalk	<input type="checkbox"/>	<input type="checkbox"/>
CBC-2 Special crosswalk	<input type="checkbox"/>	<input type="checkbox"/>
CBC-3 Vehicle turning movements	<input type="checkbox"/>	<input type="checkbox"/>
CBC-4 Removal or reduction of permanent crosswalk closures	<input type="checkbox"/>	<input type="checkbox"/>

⁵ Please check analysis in PMND to determine if design element has been cleared under CEQA. For example, as stated in p.89 of the BSP's PMND the implementation of RTOR prohibition at intersections that experience high volumes of right-turning movements (greater than 300 vehicles in the peak hour) or have near-side bus stops would require additional study and environmental review.

PROJECT NAME:

PROJECT SCREENING PART I CONT.

NUMBER/ NAME	PROJECT ELEMENT	REQUIRES SUBSEQUENT ENVIRONMENTAL REVIEW ⁶ (DO NOT FILL IN, THIS SECTION IS FOR EP PLANNER DETERMINATION ONLY)
CBC-5 Mid-block crosswalks	<input type="checkbox"/>	<input type="checkbox"/>
CBC-6 Raised crosswalks	<input type="checkbox"/>	<input type="checkbox"/>
CBC-7 Extended bulb-outs	<input type="checkbox"/>	<input type="checkbox"/>
CBC-8 Mid-block blub-out	<input type="checkbox"/>	<input type="checkbox"/>
CBC-9 Center or side medians	<input type="checkbox"/>	<input type="checkbox"/>
CBC-10 Pedestrian refugee islands	<input type="checkbox"/>	<input type="checkbox"/>
CBC-11 Transit bulb-out	<input type="checkbox"/>	<input type="checkbox"/>
CBC-12 Transit boarding islands	<input type="checkbox"/>	<input type="checkbox"/>
CBC-13 Perpendicular or angled parking	<input type="checkbox"/>	<input type="checkbox"/>
CBC-14 Flexible use of parking	<input type="checkbox"/>	<input type="checkbox"/>
CBC-15 Parking lane planters	<input type="checkbox"/>	<input type="checkbox"/>
CBC-16 Chicanes	<input type="checkbox"/>	<input type="checkbox"/>
CBC-17 Traffic calming circles	<input type="checkbox"/>	<input type="checkbox"/>
CBC-18 Roundabouts	<input type="checkbox"/>	<input type="checkbox"/>
CBC-19 Pocket parks	<input type="checkbox"/>	<input type="checkbox"/>
CBC-20 Reuse of 'pork chops'	<input type="checkbox"/>	<input type="checkbox"/>
CBC-21 Boulevard treatments	<input type="checkbox"/>	<input type="checkbox"/>

⁶ Please check analysis in PMND to determine if design element has been cleared under CEQA. For example, as stated in p.89 of the BSP's PMND the implementation of RTOR prohibition at intersections that experience high volumes of right-turning movements (greater than 300 vehicles in the peak hour) or have near-side bus stops would require additional study and environmental review.

PROJECT NAME:

PROJECT SCREENING PART I CONT.

NUMBER/ NAME	PROJECT ELEMENT	REQUIRES SUBSEQUENT ENVIRONMENTAL REVIEW⁷ (DO NOT FILL IN, THIS SECTION IS FOR EP PLANNER DETERMINATION ONLY)
CBC-22 Shared public ways	<input type="checkbox"/>	<input type="checkbox"/>
CBC-23 Pedestrian-only streets	<input type="checkbox"/>	<input type="checkbox"/>
CBC-24 Public stairs	<input type="checkbox"/>	<input type="checkbox"/>
CBC-25 Multi-use paths	<input type="checkbox"/>	<input type="checkbox"/>
CBC-26 Above-ground landscaping	<input type="checkbox"/>	<input type="checkbox"/>

OTHER DESIGN IMPROVEMENTS IN THE BETTER STREETS PLAN (BSP)
(Not identified above)

DESIGN ELEMENT NAME	BSP PAGE NUMBER	
		<input type="checkbox"/>

(EP PLANNER COMMENTS):

⁷ Please check analysis in PMND to determine if design element has been cleared under CEQA. For example, as stated in p.89 of the BSP's PMND the implementation of RTOR prohibition at intersections that experience high volumes of right-turning movements (greater than 300 vehicles in the peak hour) or have near-side bus stops would require additional study and environmental review.

PROJECT NAME:

PROJECT SCREENING PART I CONT.
(On the table below, please identify BSP's design elements that are part of the proposed project. If any of the questions listed below pertain to this project, please answer "YES". If none apply, indicate so by checking the red box below.)

IDENTIFY STORM WATER FACILITIES THAT ARE PART OF THE PROJECT

	Project Element	Requires Subsequent Environmental Review⁸ (FOR EP PLANNER DETERMINATION ONLY)
Permeable Paving	<input type="checkbox"/>	<input type="checkbox"/>
Bioretention Facilities	<input type="checkbox"/>	<input type="checkbox"/>
Swales	<input type="checkbox"/>	<input type="checkbox"/>
Infiltration Boardwalks	<input type="checkbox"/>	<input type="checkbox"/>
Infiltration and Soakage Trench	<input type="checkbox"/>	<input type="checkbox"/>
Channels and Runnels	<input type="checkbox"/>	<input type="checkbox"/>
Vegetated Buffer Strip	<input type="checkbox"/>	<input type="checkbox"/>
Vegetated Gutter	<input type="checkbox"/>	<input type="checkbox"/>
Other (describe stormwater improvements)	<input type="checkbox"/>	<input type="checkbox"/>

If none of the above BSP design elements apply, please indicate so by checking this box

(EP PLANNER COMMENTS):

⁸ Please check analysis in PMND to determine if design element has been cleared under CEQA. For example, as stated in p.89 of the BSP's PMND the implementation of RTOR prohibition at intersections that experience high volumes of right-turning movements (greater than 300 vehicles in the peak hour) or have near-side bus stops would require additional study and environmental review.

PROJECT NAME:

PROJECT SCREENING PART II
(If any of the questions listed below pertain to this project, please answer "YES". If none apply, indicate so by checking the red box below.
Note: If you answer "YES" to any of the questions listed below, this checklist may not be utilized, and therefore, and Environmental Evaluation application must be filled.)

TRANSPORTATION/CIRCULATION

Does the project include right turn on red (RTOR) at locations where the peak hour right-turning traffic volume exceeds 300 vehicles per hour; or require any removal of multiple turn lanes; or the bus stop is located in the near side?	Yes
Does the project include removal of crosswalk closures?	Yes
Does the project include mid-block crosswalks on a two-way street where traffic volumes exceed 500 vehicles per hour in either direction during the peak hour?	Yes
Does the project include roundabouts?	Yes
Does the project include pedestrian-only streets on a street where through traffic is greater than 100 vehicles per hour in the peak hour, or there is transit service, or there are driveways or parking garages, or loading activities cannot be accommodated during off-peak hours?	Yes
Does the project include multi-use paths? ⁹	Yes
Does the project include shared public ways on streets with park garages with parking spaces > 100, or through traffic > 100 cars per hours, or transit service?	Yes

PROJECT ELEMENTS THAT WILL REQUIRE TECH SPEC EVALUATION:¹⁰
(If the project includes any of the elements listed below, the project will require Tech Spec Evaluation).

HISTORICAL/ARCHEO RESOURCES
(All applications need preliminary review for potential impacts to archeological resources pursuant to EP practice.)

Is the proposed project located within a potential historic district or on a street adjacent to a historic landmark? Please state the name of the historic district or historic landmark: _____	Yes
Does the proposed project involve an identified historic resource among the following: street furniture, light standards, signage, curbs, places, bricks, walls, and other paving materials? Please identify the historic elements that are part of the proposed project: _____	Yes
Does the proposed project involve removal of trees adjacent to historic resources?	Yes

If none of the above BSP design elements apply, please indicate so by checking this box

⁹ The BSP does not provide guidance on the location or design of Multi-use Paths. Therefore, at the time a location for implementation is proposed, it would be subject to site-specific environmental review.

¹⁰ EP NEEDS TO DETERMINE HOW COORDINATION WILL OCCUR

PROJECT NAME:

PROJECT SCREENING PART III					
<i>Project elements that would require implementation of Mitigation Measures and Monitoring Reports organized by CEQA Topic.</i>					
CEQA Topic	Sub-topic	Meet criteria/threshold: ¹¹ Yes/No or N/A	Requires mitigation measure: Yes/No	Potential impacts differ from PMND analysis (Y/N). If "Yes" briefly describe on a separate sheet.	Project Sponsor Agrees to Implement Mitigation Measures
Aesthetics					
Does the proposed project involve removal of significant trees? Yes <input type="checkbox"/> No <input type="checkbox"/>	Significant trees	N/A			<input type="checkbox"/>
Does the project involve tree root trimming? Yes <input type="checkbox"/> No <input type="checkbox"/> If so, is tree root trimming greater than two inches? Yes <input type="checkbox"/> No <input type="checkbox"/>		N/A	Aesthetics Tree Root Protection Mitigation Measure M-AE-1 applies if trimming of roots are greater than two (2) inches in diameter (p.53).		<input type="checkbox"/>
<input type="checkbox"/> None of the above CEQA topics apply to the project					
Historical/Archeological Resources					
Does the project require excavation depth greater than two (2) feet? Yes <input type="checkbox"/> No <input type="checkbox"/>	Accidental discovery	N/A	Archeological Accidental Discovery mitigation measure Cul-1 applies to all projects except for those occurs in an area within Hispanic Period Archeological District (p.64).		<input type="checkbox"/>
Does the project occur in an area within the Hispanic Period Archeological District? ¹² Yes <input type="checkbox"/> No <input type="checkbox"/>	Hispanic Period District	N/A	Archeological Monitoring Hispanic Period mitigation measure Cul-2 applies (p.64).		<input type="checkbox"/>
<input type="checkbox"/> None of the above CEQA topics apply to the project					
Transportation and Circulation					
Does the project include removal of loading spaces? Yes <input type="checkbox"/> No <input type="checkbox"/>	Loading	YES	Provision of New Loading Space, Mitigation Measure TR-1 (p.78).		<input type="checkbox"/>

¹¹ The Project sponsor should discuss with EP planner how to proceed with projects that do not meet the PMND's thresholds.

¹² **TO BE EVALUATED BY EP PLANNER.** The Spanish Period Map is not available for public review due to the sensitivity of the archeological resources encountered in the area.

PROJECT NAME:

PROJECT SCREENING PART III CONT.					
<i>Project elements that would require implementation of Mitigation Measures and Monitoring Reports organized by CEQA Topic.</i>					
Air Quality					
	Construction impacts		Dust Control Plan, Mitigation Measure AQ-1 applies to ALL projects (p.120).		
Biological Resources					
Does the project include tree removal? Yes <input type="checkbox"/> No <input type="checkbox"/>	Nesting birds	N/A	Nesting Birds Mitigation Measure M-Bio-1 (p.151).		
Biological Resources (Cont.)					
What is the expected duration period of construction? _____	Nesting birds	N/A	Nesting Birds Mitigation Measure M-Bio-1 (p.151).		
Which months would construction occur? _____	Nesting birds	N/A	Nesting Birds Mitigation Measure M-Bio-1 (p.151).		
Hazardous Materials					
Does the project occur in an area within the Maher-designated area? ¹³ Yes <input type="checkbox"/> No <input type="checkbox"/>	Determination of contaminated soil	N/A	Hazardous Materials Mitigation Measure M-HAZ-1 (p.161).		
(EP PLANNER COMMENTS):					

¹³ www.sfdph.org/dph/EH/HazWaste/MaherSiteMap.asp

PROJECT NAME:

This section is to be filled by EP Planner. Use check boxes to indicate type of review conducted (as applicable). Leave blank if not applicable to the Project.

- | | |
|--------------------------|---|
| <input type="checkbox"/> | Project was screened for potential impacts to archeological resources pursuant to EP practice. |
| <input type="checkbox"/> | Project was screened by a Tech Spec for potential impacts to historical resources pursuant to EP practice. |
| <input type="checkbox"/> | Applicable Mitigation Measures are applied to the project. |
| <input type="checkbox"/> | Green House Gas analysis performed and approved by EP. |
| <input type="checkbox"/> | Air Quality Memo approved by EP. |
| <input type="checkbox"/> | The project was reviewed by DPH and DTSC, and a memo of concurrence was submitted to EP (for projects within the Maher Layer only). |
| <input type="checkbox"/> | PMND was reviewed and no items were identified that would require subsequent environmental review. |

CEQA Determination

Note to file, contingent upon regulatory agency approval or other information, as follows:

- Note to file (no additional documentation required)
- Addendum
- Supplemental EIR or MND

Notes:

Planner Signature

Signee (print name): Ryan Shum

Date: _____

Powell Street Improvement Project – CEQA Project Description

The Powell Street Improvement Project proposes streetscape and pedestrian enhancements along Powell Street in the Union Square district of San Francisco. The project aims to improve the pedestrian experience, enhance safety and cleanliness, and reinforce Powell Street’s role as a key commercial and tourist destination. Pedestrian and business access will be maintained during construction. The project is subject to funding availability, including Prop B Vibrant San Francisco Bond funds approved by more than 70% of San Francisco voters in November 2024. Additional funding sources for the project include Prop L and private funding being raised by the Downtown Development Corporation (DDC). Implementation may occur in phases based on funding availability. The project area is within the Kearny-Market-Mason-Sutter Conservation District and encompasses a section of the San Francisco Cable Car System National Historic Landmark. The project extends a short distance into the Market Street Cultural Landscape District at the southern project limit. The project’s northern limits adjoin but do not enter the Union Square California Historical Landmark.

Project Location and Existing Conditions

The project area consists of three blocks of Powell Street, extending from Market Street to Geary Blvd (Union Square). Travel Lanes: Powell Street is a primary pedestrian corridor with a mix of retail, hospitality, and transit functions, including the Powell Street Cable Car turnaround at its southern terminus. The block of Powell from Market to Ellis Streets operates as a shared street with cable cars and no vehicular traffic. The two blocks of Powell from Ellis to Geary Blvd operate as a two-way, two-lane roadway, with each lane shared by cable cars and limited vehicle traffic. Two cable car tracks run along the center of the street, serving the Powell-Mason and Powell-Hyde lines. Each travel lane is approximately 10 to 11 feet wide. Sidewalks vary between 12 and 15 feet in width. The existing streetscape includes parklets, sidewalks of varying widths, and street furnishings. The segment from Ellis St. to Market St. has no curb or gutter and is paved with ADA-noncompliant herringbone brick imitative of, but of a different material than, the brick paving used in the Market Street Cultural Landscape District.; short remnant sections of the cultural landscape district’s original characteristic wide granite curb are extant on the south side of the Ellis/Powell intersection. “Golden Triangle” light standards are extant from Ellis St. to Geary St. The area at the periphery of the cable-car turnaround is ringed with bollards and cable and is paved in ADA-noncompliant small stone pavers in contrasting shades of gray, which is also present along the borders of the cable-car trackway. The remaining sidewalk sections are conventional concrete. There are four loading zones along this street segment. The area experiences high pedestrian volumes and is subject to ongoing maintenance and security concerns.

Proposed Improvements

Pedestrian and Circulation Improvements

The existing 15-foot-wide sidewalks along Powell Street would be demolished and reconstructed with new sidewalk up to 20’ except at loading zones to be constructed at Ellis St. intersection, O’Farrell St. intersection, Geary Blvd intersection. Existing masonry paving extending to and including the existing granite band and the (4) aluminum anodized poles mounted in the granite band around the cable-car turnaround would also be removed. The pedestrian access route sections of the sidewalk, including a 12-foot band of new sidewalk circling the periphery of the existing cable-car turnaround, would be reconstructed of dark gray high-performance concrete containing 5 lbs. per cubic yard carbon-black-based concrete finish, and 25 to 30 lbs. per 100 square feet of silicon carbide sparkle grains, with

decorative metal inlay. Up to 9' of the sidewalk area near the curblines will be replaced with mortar set unit pavers similar in type and dimension but different in color to existing brick pavers along Market Street. Existing brick paving on Market Street beyond the new sidewalk around the turnaround would be replaced in-kind. Existing bollards will be replaced with standard bollards and leaning rails at the cable-car turnaround.

The widened sections, which would serve as the new street-furnishing zone, would use gray unit pavers that would conform to Public Works Order No: 200369, "Standard Paving Materials in San Francisco's Public Right of Ways." Sidewalk reconstruction would require excavation to depths of approximately 4-inches.

At intersections as determined by the Public Works ADA coordinator or as required to accommodate loading zone changes, ADA-compliant curb ramps would be installed, with new curb, gutter, and sidewalk transitions, where required. The typical excavation depth for curb ramps would be 8 inches below the surface, while locations requiring sewer lateral and catch basin modifications would require excavation up to 15 feet below grade. New ADA-compliant crosswalks would be installed, featuring high-visibility striping and special paving treatments consisting of textured concrete or thermoplastic inlay materials.

The brick roadway treatment between Ellis St. and Market Street in the cable car trackway would be repaired with the selective removal of damaged bricks and replacement in-kind. The roadway between the trackways and curbs within the rest of the project area would be ground down and resurfaced, with localized base repairs that would comprise saw cutting and removal of street base failures, compaction of the subbase and restoration of the roadway base. The top layer of asphalt of the roadway within would be cold planed and a new asphalt concrete wearing surface applied. This pavement removal and resurfacing would be entirely within the roadway prism, with no excavation required.

For all curb and sidewalk work, project will meet Public Works Order 210,865 or the most current City directives and standards. Sidewalk lights, sidewalk elevators and chutes, and utility plates that appear to be 45 years or older, preserved in place, salvaged and reset, or replaced to match existing. The work would include grinding/paving conforms, subbase repair or replacement, and installation of detectable warning surfaces.

The project area includes subsidewalk basements at the following addresses: 200 Powell St., 222 Powell St., and 226 Powell St, 242 Powell St., 246 Powell St., 247 Powell St., 235-239 Powell St., 225 Powell St., 207-217 Powell St., 236-242 Powell St., 201 Powell St., 167 Powell St., 161 Powell St., 135 Powell St., 114 Powell, 111 Powell St., 101 Powell St., 150 Powell St., 120 Powell St., and 114 Powell St., 69 Powell St., 80 Powell St, and 35 Powell Street.. These basements extend into the public right-of-way beneath the sidewalks and would be structurally impacted by the proposed improvements. Work in these locations would include reinforcement, replacement, or repair of subsidewalk basement roof slabs to ensure structural integrity and to support new sidewalk and curb extension elements. This would involve modifications to slabs, beams, and columns, as well as the integration of pole footings and street furniture foundations where applicable. To prevent water intrusion into below-grade structures, additional sidewalk waterproofing would be installed over subsidewalk basements as part of the project. Existing vaults, vents, light prisms, access hatches, and freight lifts would be preserved in place, or relocated in order not to interfere with structural improvements.

Surface and Landscape Enhancements

A distinctive “Red Carpet” decorative painted red pavement treatment would be applied to the cable car trackway within the project limits.

The project would install suspended catenary lights mounted to existing building faces and/or to new poles along the three-block corridor placed for this purpose along the alignment. These lights will be fabricated metal with a gold-tinted paint finish. At the southernmost edge of the cable-car turnaround, the project would construct a “Golden Lantern”, a larger-scale lighting installation using the same materials, measuring approximately 45feet in height and 25feet in width. The foundation for the poles for this lighting feature would be cast-in-drilled hole (CIDH) pile foundations to a depth of approximately 20 feet. No "Golden Triangle" standards would be used for lantern installation.

The project would install public seating areas within the new street-furnishing zone. These seating areas would consist of benches and integrated street furniture, placed outside the pedestrian access route to maintain an unobstructed walkway. The benches would be constructed from durable materials such as cast metal. Seating would be positioned at key locations along the corridor, including near intersections, transit stops, and commercial frontages.

The installation of seating areas would be coordinated with new planters and landscape elements, incorporating seating into expanded sidewalk zones and furnishing areas. Seating placements would avoid conflicts with existing infrastructure, such as utility access points and subsidewalk basements, and would align with accessibility standards.

Above-ground sidewalk planters would be installed in compliance with San Francisco Public Works guidelines. These planters would be no more than 36 inches in height or width and be located in furnishing zones. They would not be placed on sidewalk slopes greater than 1:12, above subsidewalk basements, or within 5 feet of street furniture such as utility poles, parking meters, or fire hydrants.

Utility and Infrastructure Modifications

The project would require modification and relocation of underground utilities, including sewer, water, power, and communication lines. Existing vaults and boxes would be relocated horizontally and vertically short distances as needed. The project would require the relocation of catch basins along Powell Street to accommodate the expansion of sidewalks and curb extensions. Existing catch basins that conflict with the new sidewalk alignment would be abandoned in place, with their associated sewer laterals decommissioned and sealed to prevent unintended drainage connections. New catch basins would be installed in adjusted locations, with new sewer laterals constructed to connect them to the existing sewer/stormwater system. Excavation for these laterals would reach a maximum depth of 15 feet below the surface.

Traffic Signal and Street Lighting Modifications

The project would upgrade street lighting and modify traffic signal posts and light poles to align with the new sidewalk configuration and curb extensions. Existing traffic signal posts and streetlight poles would be relocated or replaced, as-needed.

In addition to the new lighting installations, the project would replace, add or modify existing streetlight poles to improve illumination and integrate with the new streetscape design. Streetlights and pedestrian fixtures would be mounted on new or relocated poles, designed to complement the Golden Lantern or existing structures while maintaining compatibility with the existing utility infrastructure. The lighting system would integrate energy-efficient LED fixtures. No historic light standards will be modified, moved, or removed as part of the proposed work.

To support the new lighting and signal infrastructure, post installations would use cast-in-drilled-hole (CIDH) pile foundations, with reinforced concrete piles installed to depths of 4 to 10 feet, depending on soil conditions and pole size. If a post is installed over a subsidewalk basement, its foundation would be secured to the basement wall, with excavation extending up to 15 feet below the basement floor.

Cable Car Stop and Loading Zone Modifications

The project will relocate existing mid-block loading zones to the near-side and far-side of each block.

The project will consolidate the cable car stops at O'Farrell Street and Geary Street into two mid-block stops located between O'Farrell Street and Geary Street with one serving the northbound route and one serving the southbound route.

Construction Scope and Phasing

Construction will be dependent on funding availability and may be executed in either a full build-out or a phased approach:

- Full Build-Out (\$50 million) – Implementation of the complete design vision, contingent upon private fundraising efforts and grants.
- Phase 1 Implementation (\$30-335 million) – Based on available funding, project will focus on delivery of the following scope:
 - Golden Lantern + Catenary Lighting Focus – Prioritizing installation of the Golden Lantern at Market Street to establish a visual gateway.
 - Sidewalk Focus – Prioritizing upgrade of the sidewalk area of Powell Street
- Phase 2 Implementation will prioritize upgrade of the roadway and cable car trackway.

Project Timeline

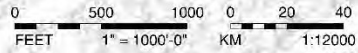
Pending funding approval and permitting, construction is expected to begin in late 2026. Construction duration is estimated at 545 calendar days. The implementation schedule will be determined in coordination with local businesses, property owners, and City departments to minimize disruptions.

City Review and Approvals

The project will require review and approval by multiple City agencies, including:

- San Francisco Planning Department (CEQA review and streetscape design compliance)
- Department of Public Works (permitting and construction oversight)
- San Francisco Municipal Transportation Agency (coordination with transit operations)
- San Francisco Fire Department (emergency access requirements)

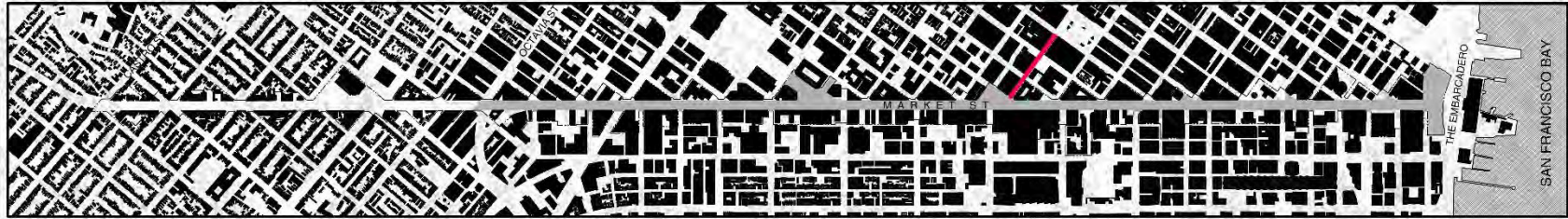
MARKET STREET CULTURAL LANDSCAPE DISTRICT



MSCLD Boundary
HALS Documentation Area

Notes:
The HALS Documentation Area is within the Market Street Cultural Landscape District (MSCLD). It includes the Market Street public right-of-way between Octavia Street and The Embarcadero and the District's plazas east of Octavia Street. Character defining features within the MSCLD that are outside of, but directly adjacent to, the HALS Documentation Area, are also included; typically these features occur where the sidewalks turn the corner from Market Street onto adjacent streets.

Project Location in relation to Market Street Cultural Landscape District



Project limits in relation to Kearny-Market-Mason-Sutter Conservation District



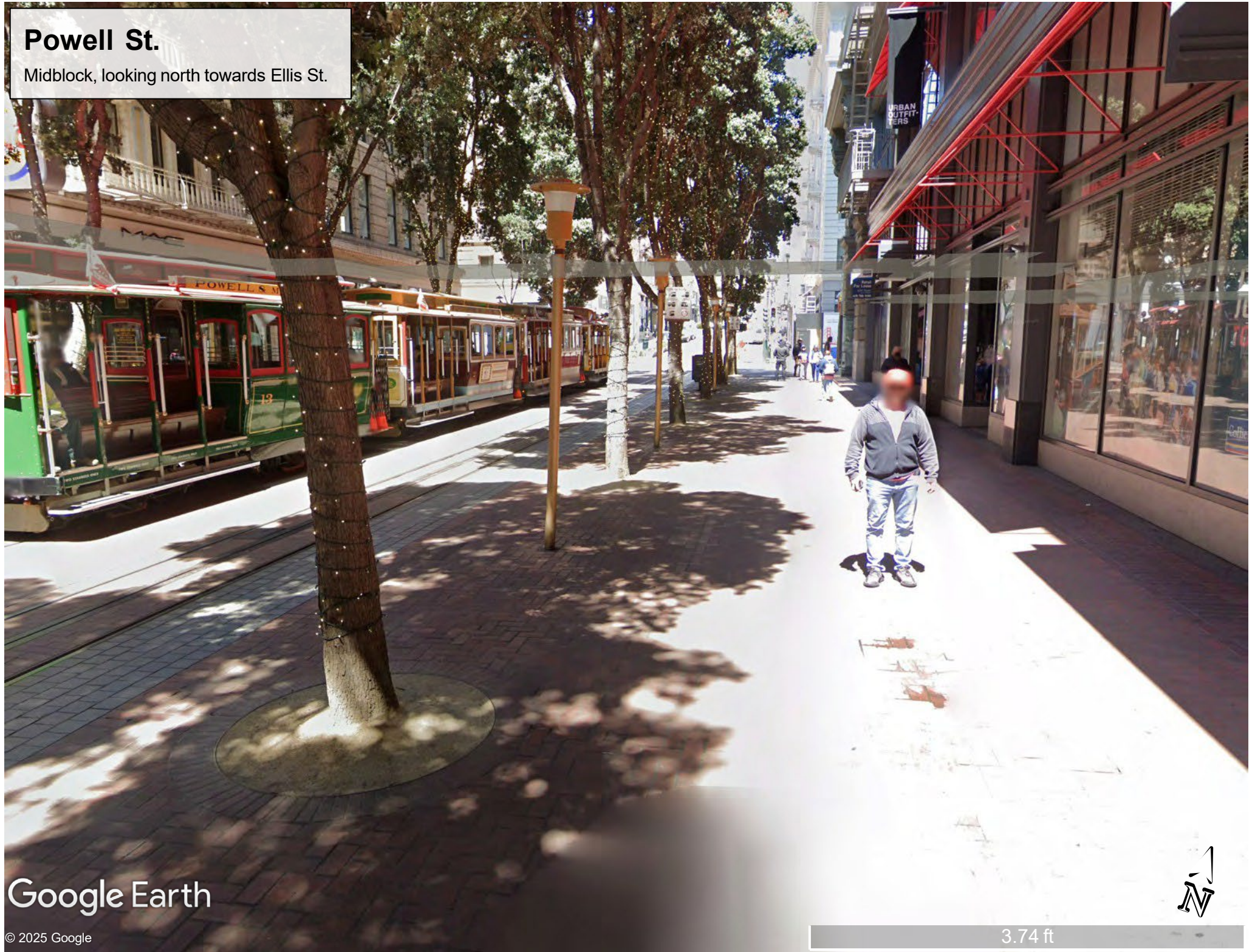
Project limits (black line) in relation to Cable Car Historic Landmark (red)

Golden Lantern Installation Concept



Powell St.

Midblock, looking north towards Ellis St.



Google Earth

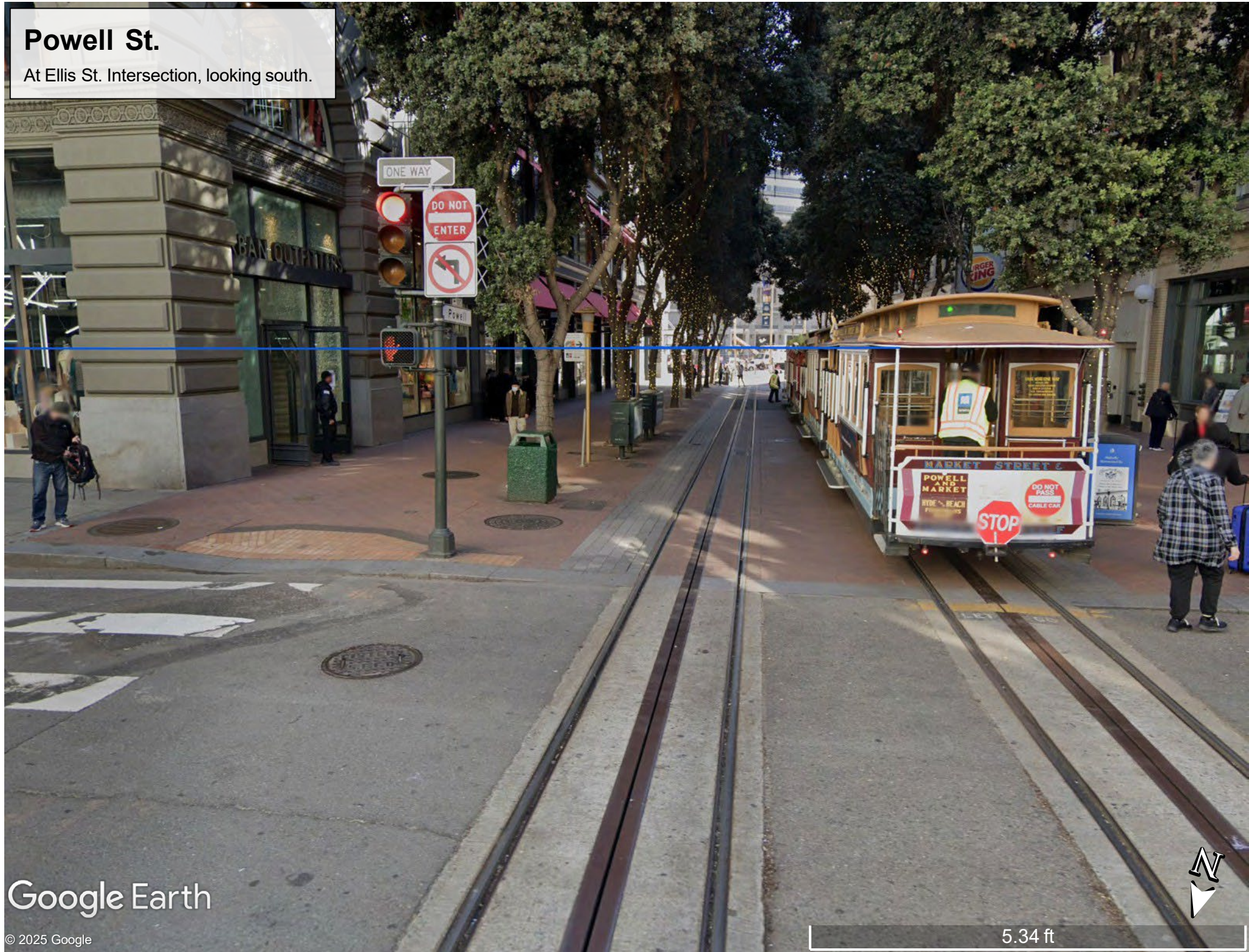
© 2025 Google

3.74 ft



Powell St.

At Ellis St. Intersection, looking south.



Powell St.

At O'Farrell St. Intersection, looking south.

ONLY
EXCEPT MUNI TAXIS
COMMERCIAL VEHICLES

HERBERT

Lids

JAPANESE RESTAURANT

UNIQLO

Life



HERBERT HOTEL

Google Earth

© 2025 Google



6.37 ft

Powell St.

At O'Farrell St. Intersection, looking North.

